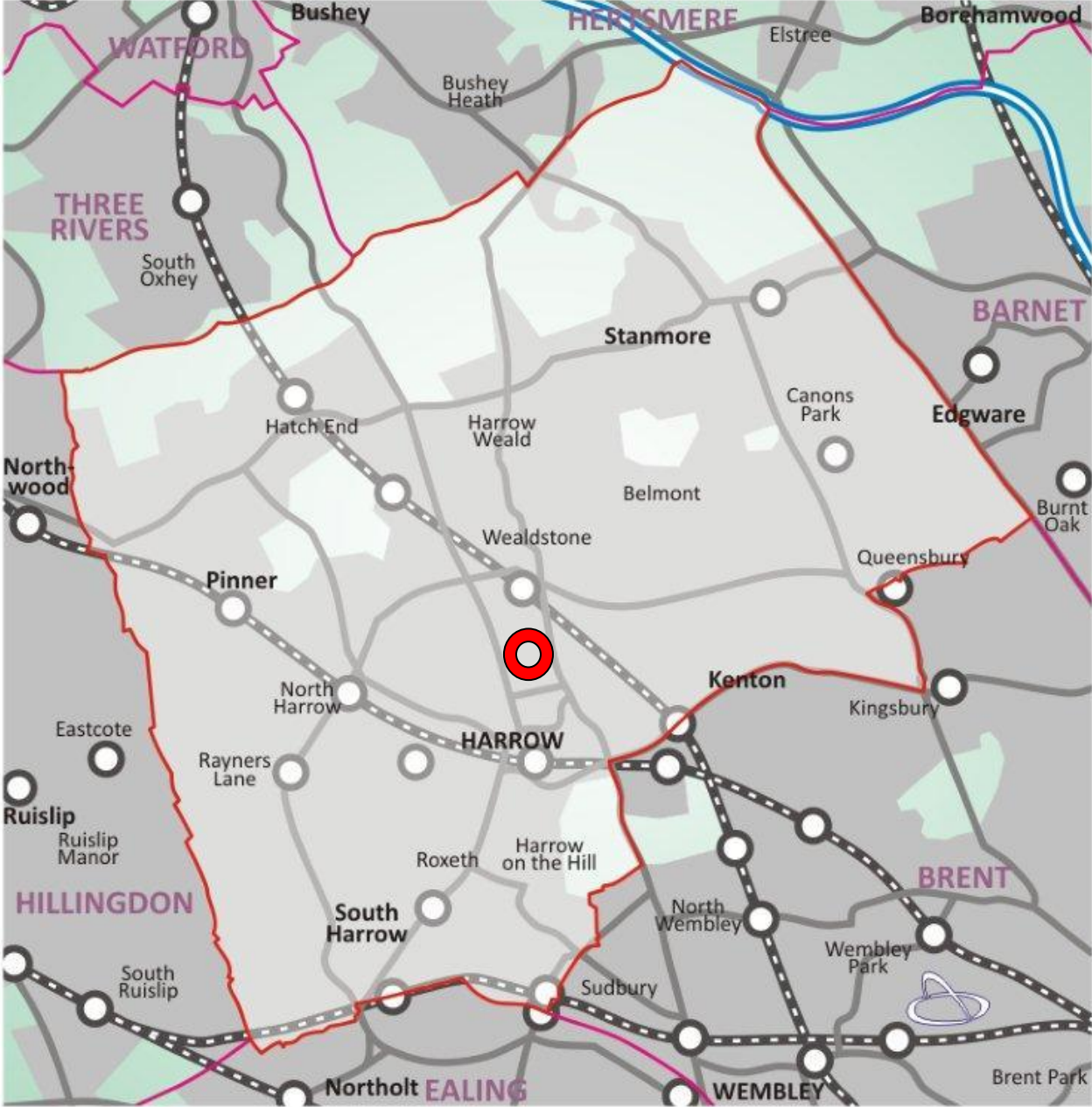
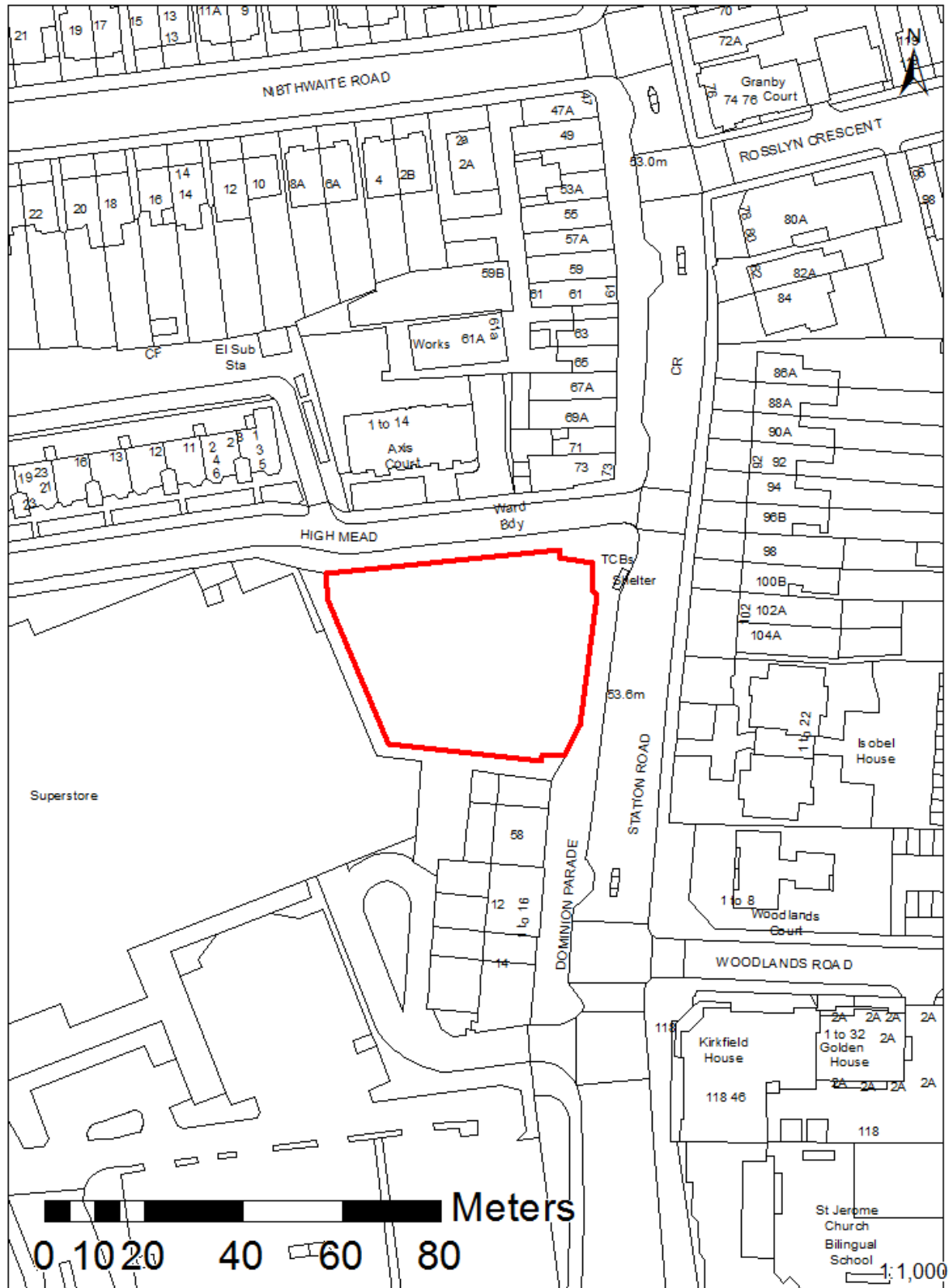


 = application site



Safari Cinema	P/3043/19
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Safari Cinema, Station Road



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LONDON BOROUGH OF HARROW

PLANNING COMMITTEE

22nd January 2020

APPLICATION NUMBER: P/3043/19
VALIDATE DATE: 02/08/2019
LOCATION: SAFARI CINEMA, STATION ROAD, HARROW
WARD: GREENHILL
POSTCODE: HA1 2TU
APPLICANT: MR GHULAM ALAHI
AGENT: MADDOX AND ASSOCIATED LTD
CASE OFFICER: Katie Parkins
STATUTORY EXPIRY DATE: 24/10/2019
EXTENDED EXPIRY DATE: 06/03/2020

PROPOSAL

Redevelopment to provide part 11/part 5 storey building with basement level comprising of 78 residential units; cinema (Use class D2) to ground floor and ancillary cafe to first and second floors; parking; landscaping; bin and cycle stores

The Planning Committee is asked to:

RECOMMENDATION A

The Planning Committee is asked to:

GRANT planning permission subject to:

1. Agree the reasons for approval as set out in this report;
2. Refer this application to the Greater London Authority (GLA) as a Stage 2 referral; and
3. Subject to the Mayor of London (or delegated authorised officer) advising that he is content to allow the Council to determine the case itself and does not wish to direct refusal, or to issue a direction under Article 7 that he is to act as the Local Planning Authority for the purposes of determining the application, delegate authority to the Interim Chief Planning Officer in consultation with the Director of Legal and Governance Services for the completion of the Section 106 legal agreement and other enabling development and issue of the planning permission, subject to amendments to the conditions, including the insertion or deletion of condition as deemed fit and appropriate to the development or the amendments to the legal agreement as required. The Section 106 Agreement Heads of Terms would cover the following matters:

Design Quality

- (a) To retain the planning submission architect/ design team for approval of all final design element in order to secure high design quality of the development.

Façade Restoration

- (A) The works of demolition or alteration by way of partial demolition hereby approved shall not commence until a method statement and sample materials to be used for the support, repairs, refurbishment, retention and attachment of the façade to the proposed superstructure shall be submitted to the council for written approval.
- (B) Notify council when cladding is to be removed. Minimum of 7 days notice.
- (C) Method statement to be updated upon removal of cladding. Submitted to council for written approval. To be approved before any further works are undertaken.
- (D) Method statement be prepared by a conservation accredited engineer.
- (E) Thereafter, the proposed works to the facade shall be completed and supervised by the conservation accredited engineer in strict accordance with the approved details.
- (F) Façade restoration needs to be completed prior to the first occupation of the flats, or the commencement of use of the cinema or café, whichever is soonest.

Affordable Housing Contribution And Viability Review

- (A) Early Stage Review
- (B) Commuted sum – £1,260,000
- (C) To pay the affordable housing contribution prior to occupation of 51% of the open market housing units
- (D) Not to occupy cause or permit occupation of more than fifty per centum (50%) of the open market housing units until the affordable housing contribution has been paid in full.
- (E) Affordable housing viability review - the late stage review cap is to be confirmed.

Delivery and Servicing Plan

- (a) Not First Occupy nor permit First Occupation of the Development until the Full Delivery and Servicing Management Plan has been Approved and

- (b) Implement and procure compliance with the approved Full Delivery and Servicing Management Plan for the duration of the beneficial use of the Development (subject to such amendments or updates as may be agreed pursuant to this schedule).
- (c) The Owner shall on the first anniversary of First Occupation of the Development and on the third and fifth anniversary of the date of Occupation submit a monitoring report to the LPA setting out and reviewing how effectively the Approved Delivery and Servicing Management Plan has operated in the previous 12 months.
- (d) The Owner shall undertake a Delivery and servicing vehicle survey within six months of First Occupation (or such other time if agreed with the LPA) and at 18 months post First Occupation and on the third and fifth anniversaries.

Travel Plans, Transport And Refuse Collection

- (a) Travel Plan Bond (to be agreed with developer) will be required to secure the implementation of all measures specified in the Travel Plans.
- (b) Travel Plan monitoring Fee - £5000 to cover a 5 year period, per Travel Plan.
- (c) Travel plan survey – For Cinema Travel Plan to be updated in the sixth month after commencement of operations including survey results and Residential Travel Plan to be updated – 75% occupied or in sixth month after occupation , whichever comes first.
- (d) Submit survey results annually with full review and update in years 3 and 5
- (e) Travel Plan Co-ordinator to be in place 2 months prior to occupation.
- (f) Should the travel plan not fulfil its agreed targets by year 5, the life of the travel plan may be extended, the cost of which will be met by the developer.

PLAY SPACE CONTRIBUTION

- (a) Children's Play Space – Contribution to be confirmed.

HIGHWAYS

- (a) Car Club Membership
- (b) Resident permit restricted.
- (c) Contribution for relocation or removal of parking bays - £5,000
- (d) S78 - new/altered access
- (e) Short stay cycle parking – supply and installation cost - £2,000

- (f) Contribution to highway improvements – to be confirmed.
- (g) Contribution towards improvements/additional capacity for public transport – to be confirmed.

Employment and Training – Construction Employment

- (a) The developer to submit to the Council for approval, prior to commencement of the development, a Training and Recruitment Plan/ Local Labour Employment Strategy. The developer to implement the agreed Plan. The training and Employment plan will include:
 - a) employment initiatives opportunities relating to the construction of the Development and details of sector delivery;
 - b) the provision of appropriate training with the objectives of ensuring effective transition into work and sustainable job outcomes;
 - c) the timings and arrangements for implementation of such initiatives and
 - d) suitable mechanisms for the monitoring of the effectiveness of such initiatives
- (b) The developer to use all reasonable endeavours to secure the use of local suppliers and apprentices during the construction of the development.
- (c) A financial contribution towards the management and delivery of the construction training programme based on the construction value of the development. This is calculated using the formula: £2,500 per £1,000,000 build cost.

Carbon Offset

- (a) £48,540 carbon offset payment prior to the commencement of development (circa 27 tonnes offset per annum)
- (b) Submission of final 'As-Built' Part L calculations confirming the carbon reductions achieved on-site and those to be offset through monetary contributions to achieve zero carbon development for the residential element and a 35% reduction for the non-residential element. If additional carbon emissions need to be offset above that already paid (i.e. £48,540 or 27 tonnes per annum), an additional contribution will be calculated on the basis of £60 per additional tonne per year to be offset x 30 years i.e. £1800 per tonne) and payable within 1 month of notification of this amount by the LPA.

Relocation Strategy - TBC

Legal Costs, Administration and Monitoring

- (a) A financial contribution (to be agreed) to be paid by the developer to the Council to reimburse the Council's legal costs associated with the preparation of the planning obligation.

- (b) A further financial obligation (to be agreed) to be paid to reimburse the Council's administrative costs associated with monitoring compliance with the obligation terms.

RECOMMENDATION B

That if, by 6th March 2020 or such extended period as may be agreed in writing by the Interim Chief Planning Officer, the section 106 Planning Obligation is not completed, then delegate the decision to the Chief Planning Officer to **REFUSE** planning permission for the following reason.

1. The proposed development, in the absence of a Legal Agreement to provide appropriate improvements, benefits and monitoring that directly relate to the development, would fail to adequately mitigate the impact of the development on the wider area and provide for necessary social, environmental and physical infrastructural improvements arising directly from the development, contrary to the National Planning Policy Framework (2019), policies 3.11, 3.12, 5.2, 5.3, 6.3, 6.9, 6.13, 7.4B, 7.5, 7.6B, 7.7, 7.8 of The London Plan (2016), policies H5, H8, D2, D11, HC1, SI2, SI3, S4, T3, T4, T5 and T6 of The Draft London Plan (2019), policy CS1 of the Core Strategy (2012), AAP4, AAP6, AAP13 of the Harrow and Wealdstone Area Action Plan (2013), policies DM1, DM7, DM12, DM13, DM14, DM27, DM28, DM42, DM43, DM44 and DM45 and DM50 of the Harrow Development Management Policies Local Plan and the Supplementary Planning Document: Planning Obligations & Affordable Housing (2013).

INFORMATION

This application is reported to Planning Committee as it is a major application. The application is therefore referred to the Planning Committee as it does not fall within any of the provisions set out at paragraphs 1(a) – 1(h) of the Scheme of Delegation dated 12th December 2018.

Statutory Return Type: E All Other Largescale Major Development
Council Interest: The Council is a freeholder of part of the site.

GLA Community Infrastructure Levy (CIL) Contribution (provisional): £250,884.00
Local CIL requirement: £564,623.55

HUMAN RIGHTS ACT

The provisions of the Human Rights Act 1998 have been taken into account in the processing of the application and the preparation of this report.

EQUALITIES

As per Section 149 of the Equalities Act 2010, a public authority must, in the exercise of its functions, have due regard to the need to—

- a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
- b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
- c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it.

The Equalities Act confirms that the protected characteristics are: age, disability, gender reassignment; pregnancy and maternity, race, religion, belief, sex and sexual orientation.

The Public Sector Equality Duty does not impose a duty to achieve a particular outcome. However, the planning process is required to demonstrate that meaningful community engagement and equalities issues have been accounted for and that diverse groups are not systematically disadvantaged by public authority processes.

According to the Victory to Victory (V2V) Community Church website, V2V is a non-denominational church comprising nominated executive board members, leaders and volunteers. It provides a number of services and functions to young parents, elderly, homeless, children and other members of the congregation.

An Equalities Impact Assessment is usually undertaken by a Local Authority in order to discharge its duty under the Public Sector Equalities Duty and inform any decisions the Council/Cabinet may make to its, policies or services. If a policy or service change shows a possible 'adverse impact' on any of the Protected Characteristic groups, the public authority must consider how this might be mitigated. However, In this case it is quite clear that there is no change to Council policy/services and that decision making is predicated on current Planning legislation and established Council and National guidance.

The applicant has submitted an Equalities Impact Assessment (EQIA) with the application. The assessment identifies 18 impacts (positive, negative and neutral) of the proposed development on people with protected characteristics. Harrow Council has an Equalities Champion for the Communities Division who was consulted on this proposal. The Champion confirmed the following "The EQIA is competently produced and fully address the issues and mitigations of displacement of sitting tenants and leaseholders and any impact on the local community"

Based on the above, in determining this planning application the Council has regard to its equalities obligations including its obligations under section 149 of the Equality Act 2010.

S17 Crime & Disorder Act

Policies 7.3.B and 7.13.B of The London Plan and Policy DM1 of the Development Management Polices Local Plan require all new developments to have regard to safety and the measures to reduce crime in the design of development proposal. It is considered that the development does not adversely affect crime risk.

OFFICER REPORT

Housing		
	Proposed Density u/ph	198 u/ha
	PTAL	5
	London Plan Density Range	215-405 u/ha
Dwelling Mix	Studio (no. / %)	3 (4%)
	1 bed (no. / %)	17 (22%)
	2 bed (no. / %)	43 (55%)
	3 bed (no. / %)	15 (19%)
	4 bed (no. / %)	0
	Overall % of Affordable Housing	0%
	Social Rent (no. / %)	N/A
	Intermediate (no. / %)	N/A
	Private (no. / %)	78
	Commuted Sum	£1,260,000
	Comply with London Housing SPG?	Yes
	Comply with M4(2) of Building Regulations?	Condition attached.

Uses		
Existing Use(s)	Existing Use / Operator	Church and Cinema
	Existing Use Class(es) s	Use Class D1
Proposed Use(s)	Proposed Use / Operator	Cinema and café and residential on upper floors.
	Proposed Use Class(es) sqm	Cinema– 1,209 sqm GIA Café – 129 sqm Residential – 8,025 sqm GIA
Employment	Existing number of jobs	0
	Proposed number of jobs	Unknown

Transportation		
Car parking	No. Existing Car Parking spaces	0
	No. Proposed Car Parking spaces	3 accessible parking spaces.
	Proposed Parking Ratio	0.04
Cycle Parking	No. Existing Cycle Parking spaces	0
	No. Proposed Cycle Parking spaces	155
	Cycle Parking Ratio	1.99
Public Transport	PTAL Rating	5
	Closest Rail Station / Distance (m)	Harrow and Wealdstone Station (1.3mi)
	Bus Routes	140
Parking Controls	Controlled Parking Zone?	Yes
	CPZ Hours	High Mead: 8:30-6:30pm Mon- Sat Rosslyn Crescent: 8:30-6:30pm Mon- Sat Nibthwaite Road: 8:30-6:30pm Mon- Sat Woodlands Road: At any time
	Previous CPZ Consultation (if not in a CPZ)	-
	Other on-street controls	Double yellow lines on Station Road
Parking Stress	Area/streets of parking stress survey	
	Dates/times of parking stress survey	-
	Summary of results of survey	-
Refuse/Recycling Collection	Summary of proposed refuse/recycling strategy	Integral refuse bins

Sustainability / Energy	
BREEAM Rating	N/A
Development complies with Part L 2013?	Yes
Renewable Energy Source / %	24.1% carbon reduction

Assessment

1.0 SITE DESCRIPTION

- 1.1 The subject site comprises a prominent detached building located on the western side of Station Road, at the junction with High Mead with bounds the site to the north. Safari Cinema was built in 1935 and consists of an intricate art deco façade attached to a building behind. The ground floor of the building is currently occupied by V2V Church and the first floor is occupied Safari Cinema.
- 1.2 The host building is not located within a conservation area nor is it listed. However, the art deco facade is locally listed. Metal cladding was installed over the façade in the 1960s.
- 1.3 Dominion Parade bounds the site to the south, and is a three-storey shopping parade consisting of shops and restaurants at ground floor level and flats above.
- 1.4 A Tesco superstore bounds the site to the west which has large areas of open car parks.
- 1.5 The building is located within the Harrow and Wealdstone Opportunity Area and in an area that has very good levels of public transport accessibility (PTAL – zone 5).
- 1.6 The site is located within the Harrow and Wealdstone Opportunity Area and the Harrow Metropolitan Centre. It does not have a designated frontage.

2.0 PROPOSAL

- 2.1 The proposed development is as follows:
 - Demolition of the building behind the façade.
 - Removal of the existing metal cladding and to reveal and restore the locally listed Art Deco façade.
 - Redevelopment of the site to provide a part 11/part 5 storey building with a basement.
 - 78 flats (3x studios, 17 x 1-bed; 43 x 2-bed and 15 x 3-bed) arranged in three blocks.
 - Communal amenity space is proposed at first and eleventh floors.
 - A new cinema consisting of 4 screens and a workshop space (Use Class D2) is proposed at ground floor level. The applicant has submitted information to demonstrate that an operator has been secured.
 - A café (Use Class A1) is proposed at first and second floors.
 - Internal refuse storage.

The current owner submitted a statement which is summarised as follows:

- The current cost of maintaining and running the building is very high. Building is very old and is not in good condition. Responsibility of staffing and maintenance is too onerous. Maintenance is a rolling project, and is very expensive as the building ages.
- We are very supportive not only of the restoration of the façade but the new cinema and café. It will be a catalyst for further redevelopment on Station Road.

3.0 **RELEVANT PLANNING HISTORY**

3.1 A summary of the relevant planning application history is set out in the table below:

Ref no.	Description	Status and Date Of Decision
P/4933/14	Change of use from bingo hall (use class d2) to combined community use (including community cafe dance classes food bank life skills training mothers and toddlers group) and place of worship (use class d1) and assembly / leisure use (use class d2)	Granted: 21/04/2015
P/0805/07	Installation of canopy and wall mounted seats on front elevation to provide a smoking shelter	Refused: 11/06/2007
LBH/2037/6	Change of use part prem. From cinema to use for indoor games - reconsideration	Granted: 22/03/1971
LBH/2037/5	Change of use of part of premises from cinema to use for indoor games	Granted: 22/03/1971
EAST/543/99/CLP	Certificate of lawful proposed development: removal of an existing external door & replacement with brickwork and internal alterations	Granted: 15/10/1999

4.0 **CONSULTATION**

- 4.1 A total of 427 consultation letters were sent to neighbouring properties regarding this application, site notices were displayed in the vicinity of the site and it was advertised in local newspapers.
- 4.2 The overall public consultation period expired on 4/10/2019. 103 objections and 21 letters of support were received, respectively.
- 4.3 A summary of the objections received along with the Officer comments are set out below:

Summary of Comments

Cinema is an iconic and historical building. The building has many original features and should be listed. The building is listed. The council should protect and preserve the building. The building should be restored. It should not be demolished to provide new flats. The independent cinema is a benefit to the community. Further details are required to regarding façade restoration and retention – must be viable. Overdevelopment of the site. Not clear how the design has developed following DRP comments.

Officer Comments

The existing building is not listed. However, the original Art Deco façade is locally listed, but the façade is currently concealed by metal cladding. The proposed development does not consist of the demolition of the entire building. The existing façade would be revealed and restored and a new building would be constructed behind the façade. The proposed development incorporates a new cinema at ground floor level which would show a range of movies, including Hollywood, Bollywood and independent movies.

The existing cinema brings character and life to the area. The cinema serves the local community and other communities. Proposal includes retention of the façade but it will not be the same. Cultural balance is important. Church is a great community facility; it provides spiritual and emotional comfort. It is an asset. Loss of the church will impact youths, elderly, homeless etc. The congregation generates business for local businesses. Should not support the loss of community facilities. Proposals would not benefit Harrow. Local residents do not support this proposal. Church should be re-provided. Insufficient D1 floorspace within the area. Church congregation is local. It serves a local need.

Officer Comments

Noted. Please refer to the section - Principle of Development for further clarification in this regard.

Summary of Comments

Submitted reports and statements are contradictory. Harrow Council should be focused on delivering public services effectively and the redevelopment of Safari Cinema is not strategic. Noise report is flawed.

Officer Comments

In relation to some issues, please note that conditions have been imposed which requires the Applicant to submit further details. Please note that Harrow Council is not the developer, but the Local Planning Authority. Therefore we have a duty to assess and determine this application.

Extensive consultation has not been conducted. Community consultation was poor.

Officer Comments

As noted in Section 4 above, neighbouring properties were notified of this proposal in accordance with our statutory requirements. In addition to this, further properties were notified who might be directly impacted by this proposal. Site notices were displayed and the application was advertised by the LPA in local newspapers, in accordance with our statutory requirements.

Needs of the users were not considered using Equalities Impact Assessments as stated in Harrow Council housing strategies and policies.

Officer Comments

Please refer to the Equalities section above.

Proposal will increase litter and rats in the area. Risk of flytipping.

Officer Comments

Please refer to the section - Character and Appearance /Locally Listed Building below which confirms that the sufficient refuse storage has been provided within the curtilage of the site.

Proposed building is too high and will block out light to neighbouring properties. Design and use of materials are unacceptable. Possible safety hazards in the building materials to be used. Negative impact on character of the area. Area is already built-up and over populated. Facilities are over stretched. Too many high-rise buildings in Harrow. Too much pollution in the area. Proposal would 'hem' in Dominion parade – broader master planning principles should be considered.

Officer Comments

Please refer to the section - Character and Appearance /Locally Listed Building below.

Please note that in relation to the safety hazards of building materials, this is not controlled through the planning system, but is controlled through the Building Regulations.

Summary of Comments

Proposed development would have structural implications to Dominion Parade. Party wall issue.

Officer Comments

Please note that issues relating to structural integrity are addressed under the Building Regulations. Issues relating to the Party Wall are addressed under the Party Wall Acts. As these are not planning issues, they would not substantiate a reason for the refusal of this application.

Insufficient affordable housing provision (11%). The Viability Report is not realistic and is flawed.

Officer Comments

Noted. Please be advised that an independent consultant was appointed to review the applicants Viability Report. Please refer to the section – Affordable Housing below.

Loss of daylight/sunlight and amenity to Dominion Parade. The heat rejection plant and plant deck is proposed in close proximity to Dominion Parade- would have detrimental impact in terms of noise, humidity and temperature. Noise and vibration from the cinema and bar. Will give rise to crime and safety. Impact to air quality. Will block boiler flue and kitchen extract at Dominion Parade. Loss of privacy, light and outlook. Overbearing and overshadowing.

Officer Comments

Noted. Please refer to the section – Residential Amenity below. Please be advised that amended plans were received removing from the heat rejection plant from adjacent to Dominion Parade and relocating it to the basement level. The Metropolitan Police Secured by Design team raised no objection to the proposal subject to a condition to obtain certification.

Recommendations for bat roosts and have green walls not been incorporated into the design.

Officer Comments

Please note that the Biodiversity Officer has reviewed the proposal and has raised no objection to the proposed development subject to conditions relating to the submission of a Biodiversity Management Plan, and the provision of a brown roof, bat roosts, bird boxes and invertebrate shelters.

Sewers are overloaded.

Officer Comments

Please note that the Drainage Department have reviewed the proposal development. They have raised no objection to the proposed development subject to a condition which would require details in relation to foul disposal to be submitted to the LPA for approval.

Summary of Comments
<p>Detrimental impact to property values.</p> <p>Officer Comments <i>Please note that the matters relating to property values are not a material planning consideration and therefore would not substantiate a reason for the refusal of this application.</i></p>
<p>New homes will add to the current problem of insufficient parking. Current use does not disrupt traffic. Additional flats will increase congestion, traffic and parking. Future residents would park in the Civic Centre car park. Inadequate parking, loading, turning, road access. Public transport is overloaded. Cycle and refuse stores would cause a disruption. Loss of on-street parking to accommodate the proposed development. What assurances will be in place to ensure residents cant obtain a parking permit. Servicing arrangements are insufficient. Parking surveys are flawed.</p> <p>Officer Comments <i>Apart from non-residential cycle spaces, all cycle stores are located within the building, alongwith the refuse stores. Please note that if planning permission is granted it would be subject to a legal agreement which would prevent future residents from obtaining a parking permit to park in residents permit bays. Please note that the Civic Centre Public Car Park consists of pay and display bays, and therefore the LPA cannot control who does/does not park there. Please refer to section - Traffic, Safety and Parking for further details.</i></p>
<p>Will cause substantial disruption during construction.</p> <p>Officer Comments <i>Please note that if planning permission is granted, the decision notice would include an informative in relation to the Considerate Contractor Code of Practice</i></p>

4.4 A summary of the letters of support received along with the Officer comments are set out below:

Summary of Comments
<p>Support the restoration of the Art Deco façade. Additional flats are welcomed as is the cinema and café. The development will act as a catalyst for change between the town centre and Poets corner. The design will set the tone and character of the type of development needed to support regeneration of the area. The Art House cinema is good for the borough and provides a cultural offering which improves on the current arrangements. Design, quality, scale and massing have been carefully considered. Attended the community engagement event and was impressed with the level of support for the scheme. Café is good for meetings and social gatherings. Cinema provides opportunity for universities and community groups to stage productions. Will improve the night time economy. Will be a gret benefit for the community. Reduced number of visitors to Safari Cinema indicates change is required.</p>

Summary of Comments
The building is an eyesore. Officer Comments <i>Noted.</i>

4.5 Statutory and Non Statutory Consultation

4.6 The following consultations have been undertaken:

4.7 External Consultation

Consultee
Secured by Design I would seek to have a planning condition submitted where this development must achieve Secured by Design accreditation. Officer Comments <i>Noted</i>
Historic England Archaeology No response received.
NHS Harrow No response received.
Greater London Authority (GLA) Comments are attached as Appendix 2. Officer Comments <i>Noted</i>
Transport for London (TFL) Comments are attached as Appendix 3. Officer Comments <i>Noted</i>

4.8 Internal Consultation

4.9 A summary of the consultation responses received along with the Officer comments are set out below:

Consultee/Summary of Comments
LBH Conservation Officer <u>Significance</u> This is highlighted partly by the local list description. In particular given the proposals put forward, it is relevant that this local list description notes that:

'The frontage was designed so that curved glazed towers holding triple fins flanked the convex central section with three tall windows, flagpoles and the name Dominion above the entrance backlit from a recess (the Dominion sign was removed many years ago)'.

Appraisal

Key heritage policies in the assessment of this proposal would be NPPF paragraphs:

- 192: 'In determining applications, local planning authorities should take account of:
 - (a) the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
 - (b) the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
 - (c) the desirability of new development making a positive contribution to local character and distinctiveness.
- 197: 'The effect of an application on the significance of a non-designated heritage asset should be taken into account in determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset'.
- 198: 'Local planning authorities should not permit the loss of the whole or part of a heritage asset without taking all reasonable steps to ensure the new development will proceed after the loss has occurred'.
- 199: 'Local planning authorities should require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible. However, the ability to record evidence of our past should not be a factor in deciding whether such loss should be permitted.

It is excellent that a proposal is being submitted to provide a viable use for this locally listed building and that the proposal is to remove the current cladding to reveal a restored Art Deco façade. This would provide great heritage benefits assuming its good condition can be revived underneath the cladding.

They are proposing double glazed units. I am concerned that this would undermine the characteristic and essential delicate curved character of the windows (see below). A recording condition would be needed for any proposal

which included demolition works. Since support for the scheme is partly based on the benefit of their restoring the façade we need to know they can do this and not have to rebuild based on whatever we approve. So, my view is that this would be needed before any approval.

Officer Comments

Noted.

LBH Economic Department

General Comments

Economic development general welcome the new employment opportunities and uses proposed for the redeveloped building, on the assumption that the proposed employment figures and related local expenditure and GVA set out in the Economic Assessment are accurate.

The new residents will undoubtedly increase expenditure in the local area while the cinema will help to attract people to the area which is welcomed.

Construction Employment

As a major application, Economic Development will be seeking construction employment opportunities on site for local residents.

This will be secured through

- (i) a training and employment plan that will be agreed between the council and the developer prior to start on site and
- (ii) a financial contribution towards the management and delivery of the construction training programme based on the construction value of the development. This is calculated using the formula: £2,500 per £1,000,000 build cost.

Employment

As well as working closely with contractors during the construction phase of the development, the Council's in house employment team (Xcite) will expect to work closely with the cinema provider in ensuring again that local residents have access to these job opportunities. This could include for example circulating the upcoming job opportunities to Xcite clients in advance of wider promotion or attending Xcite job fairs and other employment events.

Social Value

The proposals for workshops aimed at young people looking to move into a career in set design, movie making etc are welcomed. The Economic Development Team will expect the cinema provider to work closely with relevant council services (including Youth Services, Harrow Arts Centre) as well as the community and voluntary sector to maximise the benefits of these workshops and related activities.

Officer Comments

Noted.

LBH Planning Policy

Planning Policy raised no objection to the proposed development. Their comments are incorporated in the main appraisal section of the report.

Officer Comments

Noted.

LBH Environmental Health

No objection subject to the following conditions:

- 1) No floodlighting or other form of external lighting shall be installed unless it is in accordance with details which have previously been submitted to and approved in writing by the Local Planning Authority. Such details shall include location, height, type and direction of light sources and intensity of illumination. Any lighting that is so installed shall not thereafter be altered without the prior consent in writing of the Local Planning Authority other than for routine maintenance which does not change its details.
- 2) The development shall not begin until a scheme which specifies the provisions to be made for the control of noise, including low frequency and people noise emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of physical, administrative measures, noise limits and other measures as may be approved by the Local Planning Authority. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.
- 3) The rating level of the noise emitted from the site shall be at least 10dB below the existing background noise level. The noise levels shall be determined at the boundary of the nearest noise sensitive premises. The measurements and assessment shall be made in accordance to the latest British Standard 4142, 'Method for rating industrial noise affecting mixed residential and industrial areas'.
- 4) No air extraction system shall be used on the premises until a scheme for the control of noise and odour emanating from the site has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.
- 5) The development shall not begin until a scheme for the control of noise transmission to the adjoining dwellings has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of sound insulation and other measures as may be approved by the LPA. Thereafter, the scheme shall be implemented and maintained in full compliance with the approved measures.

Officer Comments

The wording of the above conditions were subsequently amended incorporation with the EH Officer.

LBH Highways Department

In preparation for heads of terms we will be seeking s106 contributions for Travel Plan monitoring (£5k) and a bond (TBA); alterations to signs and lines (£5k); maintenance agreement for on highway cycle stands (£2k) and a connection to the TfL cycleway currently in design (£3k). Permit restriction is also required .

A s278 agreement can be put in place to deal with the alterations to the crossover and implementation of cycle stands on the highway.

Transport Assessment

The Healthy Streets format encourages focus on people and how they travel. This is the opportunity to identify any issues and suggest improvements and to look at ways to encourage more sustainable travel. The submitted document goes some way to achieving this but does not go far enough. A significant part of the overall assessment is a study of the Active Travel Zone (ATZ) which consists of an area around the site within a 20 minute cycle distance. TfL has provided specific step by step guidance on how to assess the ATZ but this has not been fully applied – the key destinations for the ATZ need to be identified and reasons given for any exclusions; destinations should be prioritised based on importance and the walk/cycle routes identified and a study of personal injury accidents in these areas with suggestions for improvements; it is then necessary to map the typical characteristics of a healthy neighbourhood including other development and regeneration projects; it needs to be demonstrated how each of these characteristics will affect the ability to live car-free and how the development has been designed to respond to any issues identified. Site visits should have been undertaken and photos of the most important routes to the previously identified key destinations taken (not Google Streetview images as these may not be up to date) with statements written on how the worst parts do not meet the Healthy Streets Indicators and what improvements can be made.

To relate this to the proposal site we would expect that routes to Harrow on the Hill Station and Harrow & Wealdstone Station are studied (for example). Any issues with the routes should have been identified eg. uneven surfaces, inadequate street lighting, lack of bins, trees, benches etc. Improvements such as Copenhagen style crossings at junctions along the route, additional benches with shelter providing trees at Dominion Parade; additional lighting provided on the building near to the bus stop and along the route to the proposed residential entrances, updating existing Legible London signs to include the site, improved connections to existing or proposed cycle routes are examples of what is expected.

The biggest likely impact from this proposal is possible parking issues related to both the residential use and cinema. Whilst parking permit restriction may discourage residential car ownership, it will not stop it entirely and does not

prevent cinema users from driving to the site. Providing cycle parking does not mean that people will take up cycling instead of driving a car. The analysis required in the TA as mentioned above would demonstrate what else needs to and can be done to enable modal shift from cars to more sustainable travel.

The proposed alteration to the vehicle access will result in the loss of existing on-street parking bays. The suggestion that these can be relocated elsewhere is not helpful and suitable locations should be identified. The requested car club bay and potential 5 disabled bays are also suggested to go elsewhere but again, locations need to be identified. A car club operator will require that a bay is located close to the associated development in order to maximise its chances of success.

Cycle parking details are required – this needs to include dimensions of the storage facilities, aisle widths, ceiling heights, doorway widths etc. the design must comply with London Cycle Design Standards.

It is accepted that this proposal will see a reduction in trips in comparison with the potential for the existing site however, the introduction of 78 residential dwellings will bring about different patterns of travel and new demands (eg. overnight parking) that currently do not exist. We need to be confident that the existing infrastructure is sufficient to cope with new/different demands as the borough continues to grow. The proposed mitigation in the form of parking permit restriction, on-street cycle parking and a car club bay will all need underpinning via s106 agreement. These will help to achieve the car free lifestyle promoted by the development but further analysis of the surrounding area is required to identify whether additional measures are needed to truly enable modal shift.

Draft Construction Logistics Plan

The draft document is fine. A detailed CLP should be secured by pre-commencement condition. This must be written using the format provided with TfL guidance at www.constructionlogistics.org Should the application be granted, the developer is urged to make early contact with Network Management in order to agree any temporary traffic management measures required; these should then be included in the detailed CLP for submission.

Delivery and Servicing Plan

The draft document is fine. A detailed DSP should be secured in a similar way to a Travel Plan. Further measures should be explored in order to meet the proposed targets eg. restricting hours for deliveries, any noisy deliveries must be timed for off-peak daytime periods; produce a handbook for residents advising them of how deliveries to the site are managed and suggest alternatives eg. identify the nearest Doodle, CollectPlus or Amazon lockers. The detailed delivery and servicing plan can be secured by s106 agreement with an obligation for monitoring over a 5 year period which can be added to the TP requirements. We are not seeking a financial contribution for this

element and monitoring would be undertaken by the same people who will monitor the travel plans. We are hoping that as much of the servicing would be taking place on highway, an obligation with specific requirements that is independently monitored would demonstrate a willingness to minimise the effects on the surrounding highway. Existing loading bays along Station Road operate Mon-Sun, 7am to 7pm and the new bay in High Mead will have the same time period.

The short stay cycle parking (5 stands) can be a contribution only; we will take it on as a highways asset in any case and will maintain it ourselves - £2000.

Car Club Membership

Car-Free development.

Contribution for relocation or removal of parking bays

Travel Plan

The ATZ audit information has been included and is acceptable.

We had requested that the TPC be in position 2 months prior to occupation but this has not been specified in the document. It will be specified in the S106

Both Travel Plans will need to be revised following the baseline survey.

Officer Comments

Noted.

LBH Biodiversity Officer

The development site lies within an urban setting. Natural green space is relatively lacking with the Bonnersfield Lane Site of Local Importance for Nature Conservation more than 750 metres distant and the majority of soft landscaping within private gardens rather than in accessible public open space – with Kenton Recreation Ground, the largest of these, .

Conservation interest of the development site and its immediate vicinity appear to be currently limited to roosting potential for bats and nest sites for birds. A preliminary bat roost assessment and follow up survey have been undertaken indicating that the building was not of value to roosting bats at the time.

Provided demolition is undertaken in an appropriate manner, and any necessary safeguards taken there should be no in principle objection to the proposals on biodiversity grounds. However, all development should give consideration to biodiversity from the outset and deliver net gain for biodiversity in accordance with national, metropolitan and borough policies. The applicant seems not to have so and, if you are otherwise minded to recommend approval of this scheme, this will need to be addressed.

The landscape strategy includes a single mention of 'bio-diversity' in relation to

the herbaceous planting palette but fails to address the topic or to given more information about the proposed 'brown roof' areas. Whilst this scan treatment is disappointing in itself. The expectation is that residents would make use of outdoor leisure facilities in local green spaces, increasing pressure on biodiversity within these. No assessment of these impacts or suggested mitigation is offered.

However, the updated Design Proposals, do include a mix of trees, shrubs and herbaceous plantings which offer scope for enhancing biodiversity as well as providing interesting semi-natural green space, with some of the proposed structures able either to provide support for shelter for pollinators and other invertebrates. The ecological consultant has offered suggestions that would help deliver net biodiversity gain but these seem not to have been given consideration with the submitted proposals.

Would recommend that the following be required as conditions of any approval:

- Brown Roof
- Bat roosting/hibernation shelters
- Bird boxes
- Invertebrate shelters

Officer Comments

Noted.

LBH Waste Officer

We can collect from Station Road as it is only a few bins in this store. We assume there will be trade bins on this development with regards to the cinema complex and café, so there will be trade waste requirements.

Officer Comments

Noted.

LBH Landscape Architect

Safari Cinema, 2 Station Road, Harrow, HA1 2TU

Proposal: Redevelopment to provide Part 11/Part 5 storey building with basement level comprising of 78 residential units (3 X studios) (17 X 1 bed) (43 X 2 bed) (15 X 3 bed); cinema (Use class D2) to ground floor and ancillary cafe to first and second floors; parking; landscaping; bin and cycle stores

The proposed landscape for the ground floor, 1st and 5th floor podiums has been the subject of pre application meetings together with review by the Design Review Panel and during this process the landscape strategy has been redesigned. Broadly happy with the landscape strategy (3.1 – 3.4 Design Masterplan Principles), resident art, play and lighting strategies, all as set out in the submitted Dominion Safari design development document – July 2019. In this document an indication of the hard and soft materials palette, tree and planting strategy, boundaries, lighting, play and resident art strategy has been provided.

If you are minded to approve this application the following hard and soft landscape conditions would be required:

- Landscaping to be Approved, including landscape masterplan, all hard and soft landscape details, to include details of the planting, hard surfacing materials including decking, raised planters, watering and drainage and external seating.

Soft landscaping works shall include:

- planting plans (at a scale not less than 1:100), written specification of planting and cultivation works to be undertaken and schedules of plants, noting species, plant sizes, plant container sizes (all at time of planting) and proposed plant numbers / densities and a landscape implementation programme.

The hard landscape details shall include samples to show the texture and colour of the materials to be used, together with information about their sourcing/ manufacturer.

The hard and soft landscaping details shall demonstrate how they would contribute to privacy between the approved private terraces and the communal pedestrian routes through the courtyard gardens. Details to include to scale cross sections and elevations for the ground floor courtyard, 1st floor podium and fifth floor courtyards. Details should include tree planting / fixing details and fixing details for any climbing plants for the ground level and podium 1st floor courtyard level and any support required for the large shrubs on the 5th floor courtyard.

- Details of the brown roof which shall include a written specification and detail including the substrate..
- Details of the ground floor hard landscape for the public realm.
- Details and locations of all furniture and seating elements, swing seat, raised beds including growing medium and drainage layers and built in seating, irrigation (if proposed), pergola, screens, decking areas and sun deck loungers.
- Details, locations and general arrangement of all play elements and safety surfacing, including information about their sourcing/ manufacturer and a specification of all the play elements and surface treatment to be installed as set out in the Landscape Design development document, and arrangements for ensuring the safety and security of children using the play areas.

The development shall be carried out in accordance with the scheme so agreed and shall be retained as such thereafter.

Reason: To ensure that the development makes provision for hard and soft landscaping which contributes to the creation of a high quality, accessible, safe and attractive public realm and to ensure a high standard of design, layout and amenity.

- Boundary Treatment including decorative screening
- Levels
- Details of lighting to all public realm including locations, details of the lighting together with information about their sourcing/ manufacturer, specification, elevations, light spillage and lighting levels as set out in the Landscape Design development document.
- Details, locations and general arrangement of the sculpture and site

elements as set out in 8.0 Resident Art Strategy, in Dominion Safari design development document – July 2019, including any community engagement to develop the sculpture.

- Landscaping Scheme - Implementation including a period of 5 year period for replacements of soft landscape
- Landscape Management Plan and Landscape Maintenance including long term design objectives, management responsibilities and maintenance schedules for all landscape areas, including the ground level, 1st and 5th floor podiums hard and soft landscape, brown roof and any landscape structure for plant growth and a programme of maintenance.

A long term Landscape Management Plan for the whole of the proposed development to ensure the future success of the development, including the long term aims and objectives for the communal outdoor amenity space and play area.

Note:

A Landscape Management Plan would be expected to set out, graphically and / or in writing, the overall functional and aesthetic objectives of the landscape scheme and the steps (eg legal arrangements including ownership and management responsibilities, planned maintenance tasks, any phased works, management programme of works, monitoring procedures etc.) that will be taken after implementation to ensure that the scheme becomes successfully established and reaches maturity.

Landscape Maintenance over a 5 year period for the whole of the proposed development to ensure the future success of the development including all the hard and soft landscape.

Note:

Landscape Maintenance refers to the routine physical tasks (e.g. strimming, pruning, weeding, plant replacement, watering, litter clearance, maintenance of furniture, landscape structures and plant supports, raised beds, brown roof, play features, any decorative landscape lighting etc.) required to satisfy appropriate standards of aftercare and to enable the design and implementation objectives in respect of planting to be satisfactorily achieved. It is essential to identify who is responsible for these tasks.

A Schedule of Maintenance Operations is normally a component of a Landscape Management Plan and commonly included within a Landscape Design Specification document.

The development hereby approved shall not be occupied until a scheme for the on-going management, management programme of works and maintenance of all the hard and soft landscaping within the development, to include a Landscape Management Plan, including long term design objectives, management responsibilities and landscape maintenance schedules for all landscape areas, shall be submitted to, and approved in writing by, the Local

Planning Authority prior to the occupation of the development or any phase of the development, whichever is the sooner, for its permitted use. The Landscape Management Plan and Landscape Maintenance Plan shall be carried out in a timely manner as approved.

Officer Comments

Noted.

LBH Drainage Department

Drainage Strategy:

The drainage report submitted by the applicant is satisfactory and we have no objections to the proposals.

However, please advise the applicant that the drainage calculations included in the drainage report are not surface water storage calculations but modelling of the drainage system. Further details will be required.

· The applicant should submit a detailed drainage design in line with our standard drainage requirements attached for our approval.

· The applicant should consult Thames Water developer services **by email:** developer.services@thameswater.co.uk or **by phone:** 0800 009 3921 or on Thames Water website www.developerservices.co.uk regarding capacity of their public sewers for receiving additional discharge from the proposed development. **The Thames Water confirmation letter should be submitted.**

Basement Protection:

Please request the applicant to submit basement protection details which includes waterproofing and water tanking details.

Proposed Hardstanding

The use of non-permeable surfacing impacts upon the ability of the environment to absorb surface water, and the hardsurfacing of the front gardens and forecourts lead to localised surface water flooding. Hence our requirement for use of permeable paving for all hardstanding.

The applicant should submit full construction details of permeable paving with their maintenance plan.

We can confirm that the drainage details requested can be conditioned and is not required at the planning stage.

Officer Comments

Noted.

5.0 POLICIES

5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that:

'If regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the Plan unless material considerations indicate otherwise.'

5.2 In this instance, the Development Plan comprises the London Plan (2016) and the Local Plan. The Local Plan comprises the Harrow Core Strategy (2012), Harrow and Wealdstone Area Action Plan (AAP) 2013, the Development Management Policies Local Plan (2013), the Site Allocations Local Plan (2013) and the accompanying policies map.

5.3 While this application has been principally considered against the adopted London Plan (2016) policies, some regard has also been given to relevant policies in the Draft London Plan (2019), as this will eventually replace the current London Plan (2016) when adopted and forms part of the development plan for the Borough.

5.4 The document was originally published in draft form in December 2017 and subject to Examination in Public (EiP) with the Panel's report published in October 2019. The Mayor of London has considered these recommendations, and has either accepted them or where not, provided justification as to why accepting them would not be appropriate. The Mayor has now submitted to the Secretary of State an 'Intend to Publish' version of The Plan. It is for the Secretary of State to determine whether he agrees with the revised Plan and it ought to be published in that form.

5.5 The Draft London Plan is a material planning consideration that holds significant weight in determining planning applications, with relevant polices referenced within the report below and a summary within Informative 1.

6.0 ASSESSMENT

6.1 The main issues are;

- Principle of the Development
- Regeneration
- Impact on Protected Views
- Design and Character of the Area / Locally Listed Building
- Standard of Residential Accommodation
- Impact to Residential Amenities
- Housing Provision and Affordable Housing
- Housing Density and Unit Mix
- Traffic, Safety and Parking
- Accessibility

- Energy and Sustainability
- Contamination and Flood Risk
- Air Quality
- Statement of Community Involvement
- Conclusions

6.2 Principle of Development

- 6.2.1 The application site is currently in use as a cinema and partially occupied by a religious group (V2V), with the cinema and owner of the entire site seeking to close and vacate the site. The place of worship is noted as utilising 1562sqm of floorspace within the existing premise, and it is proposed that this would be removed, and not be re-provided for within the redevelopment of the site. The existing cinema has a floor area of 3100sqm, which would be replaced on site by 1462sqm of new cinema space. The proposed development would therefore result in the loss of a place of worship, and also a reduced quantum of assembly and leisure.
- 6.2.2 The London Plan (2016) through Policy 3.16 (Protection and Enhancement of Social Infrastructure) seeks to ensure that planning decisions by resisting the loss of social infrastructure in areas of defined need for that type of social infrastructure without realistic proposals for re-provision. This is carried through in the draft New London Plan (2019) by way of Policy S1 (Developing London's social infrastructure). Policy S1 resists the loss of social infrastructure in an area of defined need.
- 6.2.3 The application site, whilst not specifically allocated, is located within Site 10: High Road Opportunity Area, as detailed within the Harrow & Wealdstone Area Action Plan (2013). This site is noted as being the area of Station Road that links Harrow Town Centre with Wealdstone Town Centre. This sets out a number of key site objections for this stretch of Station Road. Whilst the Safari Cinema is not expressly identified within this allocation, it is so within the Station road Sub-Area, and notes that the removal of cladding and restoration of the front of the Safari Cinema is sought. Furthermore, Policy AAP2 which relates specifically to Station Road, seeks at AAP2D to realise any opportunity that emerges to remove the existing cladding from the Safari Cinema building and restore the Art Deco façade. It is clear that there is strong policy support for a scheme to come forward at the site, which would in part, provide a development that ensures the exposure of the original Art Deco façade of this building.

Cinema Use

- 6.2.4 Policy DM40 of the DMP supports proposals for a mixed use development within town centres. Residential uses add to levels of activity within centres after normal trading hours and can support the evening economy, as well as increasing levels of natural surveillance. Social infrastructure contributes to the creation of vibrant places at the heart of local communities, particularly in areas which are well served by public transport. Policy DM41 of the DMP supports proposal which enhance the evening economy of town centres. The supporting commentary for

the Policy AAP2 also notes that the Safari Cinema is a valuable community asset. It is noted that within Harrow, there are very few cinemas. Within Harrow there are only two Cinema facilities, being at the application site and within the St Georges Shopping Centre. The proposed development would result in a reduction in cinema floorspace (Use Class D2). However, whilst there would be an overall reduction, the cinema would operate with four screens showing a wider range of types of films. The GLA in their Stage 1 response strongly supports the re-provision of a new cinema and was satisfied that the applicant has demonstrated that the proposed Art House cinema would compromise a comparable commercial offering, improving on the existing cinema layout which comprises a significant amount of redundant and inefficient floorspace. The proposed cinema use is considered to be appropriate and weighs in favour of the application.

- 6.2.5 The applicant submitted an Economic Statement which confirms a net improvement to employment in generation through the creation of 9 additional jobs, in accordance with policies 2.15 of The London Plan (2016), policies SD6 and SD7 of The Draft London Plan (2019) and policy CS1.P of the Core Strategy (2012)

Loss of a Place of Worship

- 6.2.6 The place of worship floorspace is operated currently by V2V (Victory 2 Victory), a non-denominational church. It is noted that under that planning permission which permitted this use (P/4933/14), the supporting planning statement notes that the existing congregation for V2V is located elsewhere in London. It goes on to state that the total congregation attending the other premises totals 500 churchgoers which will continue to attend at the new premises. It is not clear what number of local Harrow residents attend the existing V2V church. It is understood that throughout the current application, information on congregation has not been forthcoming. It is not clear that the existing use is located within the community which it intends to serve.
- 6.2.7 Policies 3.16 of The London Plan (2016) and S1 of The Draft London Plan (2019) seek to protect and enhance social infrastructure provision, including faith facilities, and resists proposal that would result in the loss of social infrastructure in areas of defined need for that type of facility without realistic proposal for re-provision.
- 6.2.8 The GLA was consulted on this proposal and in their Stage 1 response it was stated “*..in the absence of a commitment to re-provide the existing church community uses on the application site, a detailed relocation strategy must be provided for the existing church use prior to enable the GLA to confirm the acceptability of the sites redevelopment. Both its strategy and implementation must be secured through conditions of approval and s106 obligations attached to any approval*”.
- 6.2.9 Policy S1.A of The Draft London Plan (2019) states “*when preparing Development Plans, boroughs should ensure the social infrastructure needs of London’s diverse communities are met, informed by a needs assessment of social infrastructure. Assessments should consider the need for cross-borough collaboration where*

appropriate and involve relevant stakeholders, including the local community". However, as both The London Plan (2016) and The Draft London Plan (2019) post-dates the DMP, the LPA has not conducted such a needs assessment and therefore the need for such a facility is undefined. Therefore, there is no policy basis to request a Relocation Strategy. On this basis, it is considered that policies 3.16 of The London Plan (2016) and S1 of The Draft London Plan (2019) are not relevant to this proposal. However, notwithstanding this, the Applicant is intending to submit a Relocation Strategy. At the time of writing this report, the Relocation Strategy was not available, however an Addendum shall be issued to this report to provide further commentary in this regard.

- 6.2.10 Policy DM47 of the Harrow Development Management Policies Local Plan (2013) provides criteria which, when complied with, allow a loss of such community floorspace. The supporting planning statement provides an assessment of this policy. It is considered that the proposed development, resulting in the loss of the D1 floor space, would find favour with Policy DM47 (b) and (d).
- 6.2.11 With regard to DM47(b), the applicant notes that there are a number of similar facilities, being places of worship, within the area surrounding the site. Indeed there are a number of D1 uses that are located within the boundary of the Harrow & Wealdstone Opportunity Area, offering adequate similar facilities.
- 6.2.12 Further to the above, the proposed development would find favour with Policy DM47(d), whereby the development would secure an over-riding public benefit. In determining the over-riding benefits the scheme, the planning statement highlights what these would be, including the economic benefits and a cinema offer that appeals to a wider audience than that which exists. Their points would hold some weight. The proposed development would also provide 78 residential units towards the boroughs housing stock, which would be welcomed. Furthermore, there would be an Affordable Housing Contribution (Circa 11%), of which there is an identified need. Whilst a welcomed addition to the housing stock, the housing offer (including Affordable Housing Offer) would not constitute an over-riding benefit on its own, nor in conjunction with the matters raised above. The proposed housing provides a public benefit, but it is noted that the affordable housing offer falls short of the borough wide target (40%), and is also of a tenure that is not of the greatest need to the borough. Whilst the maximum reasonable affordable housing may be delivered by the scheme (as determined via the Financial Viability Assessment), this would be a requirement in any case of a development and secured by a S.106. However, the above in conjunction with the removal of the temporary cladding that currently exists on site, and the making good of the listed façade, the scheme would provide an over-riding public benefit. This is discussed further below.

Removal of Temporary Cladding / Façade Restoration

- 6.2.13 There are multiple references within the Area Action Plan (2013) to have the temporary cladding on the front façade of the building to be removed, and the restoration of the front of Safari Cinema. The proposed development would remove this and restore the front elevation as part of the scheme. This is supported and encouraged, and holds significant weight in considering an over-

riding public benefit. These works (in conjunction with the other matters raised above) would result in an over-riding public benefit, and should be secured by way of an appropriate legal obligation.

- 6.2.14 It is considered that the proposed development, when concluding on all benefits of the scheme, would result in an over-riding public benefit. Specifically, by reason of the removal of the temporary cladding and the restoration of the front elevation of the Safari Cinema building. A S.106 obligation should be sought and entered into to secure the removal of the temporary cladding and restoration works, as this would go to the heart of the planning permission. Subject to the above, the principle of the proposed development is considered to be acceptable.

6.3 Regeneration

- 6.3.1 It is inevitable that the character of the area will significantly change through the as a result of the high density of development proposed. However, the increase in density in this location is vital to support the wider regeneration of the Heart of Harr Opportunity Area and its surrounding area through sustained economic growth and job creation.

6.4 Impact on Protected Views

- 6.4.1 The application site sits within the Stanmore Country Park viewing corridor which provides a long distance panorama across Greater London. A number of key features are visible, including The Shard, Wembley Stadium, Harrow on the Hill and St Marys Church. The application was accompanied by a Townscape and Visual Impact Assessment. The height of the proposed building would not exceed the maximum height allowed above ordinance datum within the main protected viewing corridor. Whilst noting that there will be a change within the townscape views, the changes to the landscape panorama associated with the form of development proposed would not erode the fundamental qualities of these views. The scale and development typologies would instead reflect the sites strategic location, within the Heart of Harrow AAP area.
- 6.4.2 Based on the above, the proposed development complies with the National Planning Policy Framework (2019), policy 2.15 of The London Plan (2016), policy SD6 of The Draft London Plan (2019), policies AAP2 and AAP8 of the Harrow and Wealdstone Area Action Plan (2013), policies CS1.L and CS1.P of the Core Strategy (2012) and policies DM3, DM47 of the Development Management Policies Local Plan (2013).

6.5 Design and Character of the Area / Locally Listed Building

- 6.5.1 Safari Cinema was built in 1935 and consists of an intricate an art deco façade attached to a building behind. The façade is locally listed and the local list description notes that: 'The frontage was designed so that curved glazed towers holding triple fins flanked the convex central section with three tall windows, flagpoles and the name Dominion above the entrance backlit from a recess'. Although, the Dominion sign was subsequently removed. It is understood the since construction, it has undergone a number of refurbishments including internal

alterations in 1972 when the single large auditorium was divided into smaller screens and the lower level made into a bingo hall, which was subsequently converted into a church.

- 6.5.2 The proposed development would see the removal of the temporary metal cladding which was fixed to the upper section of the façade in the 1960s, and to reveal and restore the Art Deco façade.
- 6.5.3 A new building would be constructed behind the façade which would consist of a 5 storey wing to the north set behind the facade and an 11 storey element to the rear, approximately 35m in height and set-back from the façade by approximately 30m. The taller element facing Station Road would have white render to compliment the Art Deco façade. Only this taller element would be higher than the existing building, however it would not be readily visible from Station Road. The existing Art Deco fins would have sufficient clear space around them to ensure they are the prominent features which were they originally intended to be. The proposed taller element would not dominate the Art Deco façade, which is essential in order to maintain the architectural integrity of the façade.
- 6.5.4 It is noted that Tesco's have aspirations to redevelop their existing superstore located to the west of the application site. However, at the time of writing this report, a formal application has not been submitted for the Tesco redevelopment. However, the Applicant has worked with emerging designs of the adjacent Tesco site, to ensure the proposed redevelopment of Safari Cinema would not prejudice Tesco's proposals. The GLA Stage 1 response confirms that the proposed taller element in acceptable in terms of massing and building distances.
- 6.5.5 The facing materials for the proposed building would be red stock brickwork with horizontal, reconstituted stone banding on the flank walls to reference the banding on the existing building. It is considered that this banding would soften the proposed red-stock brickwork and would tie-in the proposed new building to the existing façade. However, a condition is appropriate requiring samples and further information on specific detailing of the materials.
- 6.5.6 A basement would be provided to the rear of the site to accommodate plant rooms and a cycle store and would be accessed from High Mead. Between Dominion Parade and the southern flank wall of the existing building, there is an existing passageway which would be infilled, but set-back from the Art Deco frontage.
- 6.5.7 The design of the proposed development has been developed through engagement with the Council and following advice from the Design Review Panel and the GLA Urban Design Officer. All parties were satisfied with the dimensions, proportion and form of the residential tower element and welcomed the horizontal banding, in addition to the activation of the façade by creating a first floor café.
- 6.5.8 Accordingly, when considering the scale and massing of the surrounding development and streetscape, the height and proportions of the proposed development are considered to be acceptable. The proposal would make a significant and positive contribution to the streetscape and is of a scale, form and design which is appropriate to the site, the Art Deco facade and its surroundings.

Furthermore, it is noted that GLA Stage 1 response confirms that the height, massing and materiality of the scheme is broadly supported.

- 6.5.9 GLA in their Stage 1 response advised that there should be a proposal in place for the ground level frontage of the podium to avoid creating inactive frontage onto the adjacent residential scheme and or/its public realm. The applicant has worked with the emerging designs of the adjacent Tesco site, which could potentially include a landscaped area directly adjacent the western boundary of the subject site. At ground floor level, to the rear of the building is a courtyard area, which has been designed to respond to the Tesco scheme, if Tesco do pursue their redevelopment proposals. Furthermore, the Applicant has advised that this courtyard area can be redesigned if necessary in order to improve its relationship with the Tesco proposal, if necessary. It is not considered appropriate to provide an active frontage for the Cinema facing towards the rear of the site, towards Tesco. The original intention for the Art Deco façade was to provide a grand entrance to the Cinema. It is considered that the incorporation of a cinema frontage to the rear of the building, would compromise the intention and integrity of the façade. Furthermore, the cinema has been laid out to ensure all back-of-house functions, including dressing rooms, office, plant etc are located to the rear behind the screen rooms, in order to provide an attractive foyer for the cinema and a flexible gallery/screen/workshop area along the High Mead elevation.

Façade Restoration

- 6.5.10 Policy DM7 (A) of the DMP supports proposals for the preservation, conservation or enhancement of a heritage asset and its setting, or which secure opportunities for sustainable enjoyment of the historic environment. The survival of heritage assets is usually best supported by viable economic uses which are compatible with the character and fabric of the asset which secure their repair and on-going maintenance in the long term.
- 6.5.11 CalfordSeaden were appointed by the Applicant to undertake an inspection and provide advice in relation to the façade retention and restoration. Calfordseaden have advised that the cladding is supported on a steel framework behind the cladding, which is supported off the canopy over the main entrance areas and tied back to the façade at high level only. In some locations, the steel framing to the over-clad façade was built into the loadbearing masonry. It was also advised that the façade appeared to be constructed in brickwork of one brick thickness and would not be capable of being self-supporting once the building behind it is demolished. Therefore, a temporary frame will be required to provide restraint to the façade during the construction activities. However, once the new building is constructed, the new frame behind may be designed to provide lateral restraint to the façade.
- 6.5.12 The report provided by Calforseaden November 2018 concludes 'there is no evidence of extensive structural damage or deterioration that would prevent retention of the main façade and its incorporation into a new development provided temporary works are carefully and sympathetically designed to support the façade during the construction process'.

- 6.5.13 At this stage, it would not be reasonable to request a detailed Method Statement for the restoration of the façade, until the existing cladding is removed and the façade re-surveyed in detail. The Method Statement should be based on the existing conditions following a comprehensive survey. The proposed development provides the opportunity to reveal the significance of the façade and to facilitate its restoration. It would make a significant and positive contribution to the streetscape and will allow the public to enjoy it. The restoration of the façade is an essential element of the proposed development and this in itself is a public benefit. Therefore, it is considered appropriate to include an obligation in a S106 Agreement to ensure a Method Statement, which is prepared by a suitable qualified person is to be prepared prior to the removal of the cladding, and is to be updated following removal of the cladding. It is also considered appropriate to ensure the façade restoration is completed prior to the first occupation of the flats, or the commencement of the use of the cinema, whichever is the soonest.
- 6.5.14 The existing façade contains single glazed, Crittal windows. During the course of this application, the Applicant commissioned and submitted a Window Condition Report. The report confirms that the windows of the building are fabricated from mild steel which has been brush coated using air drying paint. The frames feature narrow 35mm, profiled glazing bars and are glazed internally with single glass retained with metal casement putty. The report confirms that the frames exhibit evidence of severe corrosion both visible and concealed as well bowed and deformed metalwork. Red rust is apparent on many sections such that the applied coating has cracked and lost adhesion to the steel substrate, both externally and internally. On most windows the hinges are in poor condition and are rusting. In addition, there is a window wall section on the left-hand side of the building which has collapsed with obvious effects to the existing frames. The report recommends that either the original windows are refurbished and new single glazing is installed, or the replacement of the windows with glazing bars and double glazing. The Applicants preference is the replacement of the original single glazed windows with new like-for-like double glazed units. However, the Conservation Officer has concerns that double glazed units would undermine the characteristics and essential delicate curved character of the windows. There are also concerns other the feasibility of curved double glazed units. Therefore, a condition is considered to be appropriate to ensure details of windows and doors, including samples are submitted to the LPA for approval.
- 6.5.15 The Conservation Officer was consulted on the proposal and has raised no objection to the proposed development, subject to conditions in relation to recording; restriction on the installation of security measures, grilles alarms etc and further details in relation to windows in the façade.

Landscaping

- 6.5.16 The proposed development occupies a similar footprint to the existing building. Given the significant site coverage the proposal would leave minimal space for meaningful soft landscaping at grade. However, the proposed development has incorporated a courtyard area at ground floor level in association with the community use and communal amenity areas at first and fifth floor levels, for use by the future occupiers of the proposed flats. In the GLA Stage 1 response a

concern was raised that users of the café would be able to access the first floor communal amenity area. Whilst a door is shown on the floorplan for the café leading out to this communal amenity area, the Applicant has confirmed that this door would be for residents only, to provide them with direct access to the café and Station Road. The proposal is therefore acceptable in this regard. A condition is appropriate which requires the Applicant to achieve Secured by Design certification, which would ensure the appropriate security measures and access control systems to prevent unauthorised access to residential areas.

- 6.5.17 Given the minimal areas for landscaping, the Council will be seeking a high standard of soft landscaping and planting to provide a high standard of amenity. A comprehensive landscaping strategy was submitted, which provides details in relation to the proposed soft and hard landscaping, boundary treatments and play equipment. The areas of hardstanding must be finished to a high standard, consisting of a palette of materials that complements the building. The Landscape Architect was consulted on the proposal, and raised no objection to the proposed development, subject to conditions in relation to the submission of a landscape masterplan, including soft and hard landscaping, a landscape management and maintenance plan, details of the boundary treatments and levels. The appropriate conditions have been added accordingly.
- 6.5.18 Policy G5 of The Draft London Plan (2019) boroughs to develop an Urban Greening Factor (UGF) to identify the appropriate amount of urban greening required in new development. In the interim, the Mayor recommends a target score of 0.4 for developments that are predominantly residential. In accordance with the advice received from the GLA in their Stage 1 response, the applicant provided further details on the UGF for the proposed development. The UGF for the proposed development is 0.08, contrary to policy G5 of The Draft London Plan (2019). However, due to the footprint of the proposed development and the retention of the existing locally listed façade, there are limited opportunities at grade for successful planting. The Councils Landscape Architect advised that due to the layout and height of the proposed building would not facilitate extensive areas of planting to be successful. Therefore, a Landscape Strategy was produced which focused on providing functional areas to ensure a high standard of amenity for future residents. This includes areas of decking and hard surfacing with sun loungers, seating, dining areas, play equipment etc. Therefore, taking into account the site circumstances and the proposed Landscape Strategy it is considered that the insufficient UGF would not warrant a refusal of this application.

Biodiversity

- 6.5.19 The application was accompanied by a Biodiversity Report. The Biodiversity Officer was consulted on the proposal and raised no objection to the proposed redevelopment subject to conditions relating to the submission of a Biodiversity Management Plan, and the provision of a brown roof, bat roosts, bird boxes and invertebrate shelters.

External Lighting

- 6.5.20 The applicant has submitted a lighting strategy which confirms its aim is to provide an attractive, welcoming and safe space for both residents and visitors. Given the nature of the proposed development, the lighting strategy consists of bollard lighting, up-lighters-, in-ground lights and directional direct lights within all three communal amenity areas. The Landscape Architect raised no objection to the proposed strategy, however a condition is considered to be appropriate to ensure further details of the proposed lighting, including light spillage and lighting levels is submitted to the LPA for approval.

Refuse

- 6.5.21 Separate waste and recycling facilities are proposed for the residential units and the community use. As per the proposed ground floor, waste and recycling facilities for the residential units within Block A would be within an internal refuse store accessed from High Mead. An additional two internal refuse stores would be accessed from Station Road and would provide waste and recycling facilities for residential units within Blocks B and C.
- 6.5.22 The Council's Waste Management Officer reviewed the proposal and is satisfied that an appropriate level of refuse storage would be provided and would be accessible by the refuse collectors. Therefore, the proposed refuse arrangements would not result in a detrimental impact to the character and appearance of the area and therefore complies with policies DM1 and DM45 of the Development Management Policies Local Plan 2013.

Fire Strategy

- 6.5.23 The application was not accompanied by a Fire Strategy. In accordance with policy D11 of The Draft London Plan (2019) it is considered appropriate to include a pre-commencement condition which requires a Fire Strategy to be submitted to the Council to demonstrate how the proposal would achieve the highest standard of fire safety, reduce risk to life, minimise the risk of fire spread and providing a suitable and convenient means of escape for all users of the building. Such a condition has been included.
- 6.5.24 In conclusion, the proposed development, subject to the imposition of appropriate conditions and planning obligations would achieve a high quality form of development which would result in the restoration of the locally listed Art Deco façade and would add positively to the built form, setting out a high quality design which would substantially contribute to the character and appearance of the area. Sufficient refuse storage would be provided within the curtilage of the site and the proposed development would meet the high quality design and landscape aspirations of the National Planning Policy Framework (2019), policies 7.4B, 7.5, 7.6B, 7.7, 7.13, 7.19, 7.21 of the London Plan (2016), policies D1, D2, D7, D8, D9, D11, G5, G6 and HC1 of The Draft London Plan (2019), policy CS1 of the Harrow Core Strategy and policies AAP4, and AAP6 of the Harrow and Wealdstone Area Action Plan (2013) and policies DM1, DM7, DM21, DM22, and DM45 of the Development Management Policies Local Plan 2013.

6.6 Standard of Residential Accommodation

Space Standards

- 6.6.1 The majority of the proposed flats are shown to exceed the minimum space standards. 86% (67 units) of the proposed flats would have a gross internal area which exceeds the minimum standards. 14% (11 units) of the proposed flats would have a gross internal area which is as per the minimum standards.
- 6.6.2 The individual rooms within the flats are of good layout and size and suitable internal circulation space is provided in all units. However as noted above, there are 3 units A1.04; A2.04 and C3.02 which would have a shortfall. Flats A1.04 and A2.04 would have a shortfall of 1sqm and duplex flat C3.02 would have a shortfall of 3sqm. The units in all other regards would have good internal layout and would be dual aspect units. Furthermore, the duplex flat C3.02 would have access to a private terrace of approximately 52sqm, thereby providing a high standard of amenity.
- 6.6.3 It is considered that the shortfall in the floorspace as noted above would have no discernible impact on the quality of the accommodation provided to warrant a refusal on such basis. In this respect, the proposal is considered acceptable. The development would also achieve the minimum floor to ceiling height of 2.5 metres as required by the Housing SPG.
- 6.6.4 The SPG requires built in storage space to be provided in all new homes. Insufficient storage space has been provided as per the minimum standards of policy 3.5C of The London Plan (2016), however as noted above the majority of the proposed flats have a gross internal area which exceeds the minimum standards and therefore it is considered that there would be sufficient space to provide additional storage space, should a future occupier require it. It is therefore considered that this would not warrant a refusal of this application.

Noise and Disturbance

- 6.6.5 There are a number of flats located at first floor level which would be sited, wholly or partially above the bike and refuse stores and areas within the cinema, including the workshop, gallery, Screen 04, plant room, office and food prep etc. Due to the building configuration, a number of flats would have bedrooms sited adjacent to living/ kitchen areas of adjoining flats. In the case of the cores to each of the buildings, a number of habitable rooms would be sited adjacent to the lift shaft. Whilst this is not ideal, in most cases due to site constraints, this is unavoidable. However, having regard to the fact the development would be a new build, the proposal would be required to ensure that sufficient noise insulation is provided to meet Building Regulations Part E, which would ensure that any noise impact is minimised. When considered against the requirement for thermal installation also, it is considered that sufficient level of noise mitigation would be achieved to provide a good level of accommodation for future occupiers.
- 6.6.6 The applicant submitted an Environmental Noise Assessment. It is noted that Dominion Parade is directly to the south and would adjoin the proposed building.

As noted above, there are a number of other residential properties within the vicinity, located to the north of the site along High Mead and locates to the west of the site along Station Road. The first floor communal amenity space would be predominantly screened by the building and as identified in the submitted Environmental Noise Assessment, the ambient noise levels would most likely fall sufficiently below the BS 8233 criteria. Furthermore, the rooftop amenity area would have no direct line of site to the road and would also benefit from screening effects.

- 6.6.7 The Environmental Noise Assessment indicates that the existing ambient noise levels are below the BS 8233 threshold criteria for internal ambient noise levels with the windows closed. However, the Assessment identifies that the external noise levels for the balconies/terrace overlooking High Mead would exceed the BS criteria. However, BS 8233 recognises that compliance with the threshold criteria might not be achievable in all circumstances, such as urban areas adjoining the strategic transport network and therefore a compromise between elevated noise levels and other factors, such as sustainable development and the use of brownfield sites might be warranted. In this particular case, given the sites location directly adjacent to Station Road it is considered that the development has been designed to achieve the lowest practicable levels in these external spaces by ensuring there is no direct line of sight from balconies/terraces to Station Road.
- 6.6.8 Furthermore, the Environmental Health Department was consulted on the proposal and have raised no objection to the proposed development, subject to conditions restricting cinema and café opening hours, and conditions which require further details in relation to noise and vibration mitigation to be submitted to and approved by the LPA.

Private open space

- 6.6.9 The SPG requires a minimum of 5sqm per 1-2 person dwelling and an extra 1sqm for each additional occupant. 4 units (3x studios and 1x1bed unit) would not be provided with a balcony but would have a Juliette balcony. All other 74 units would be provided with balconies or a terrace area, which either meets or exceeds the guidance provided in the SPG. The GLA Stage 1 response requested that the 1-bed flat (B3.02) with a Juliette balcony should be oversized to compensate for having no external amenity space. However, in addition to the private balconies/terraces, residents would have access to a communal amenity space located at first and fifth floors, which is also consistent with the guidance contained in the Mayors SPG. Therefore, on balance it is considered that the lack of private amenity space for approximately 4% of the units proposed, would have no discernible impact on the amenities of the occupiers of these units.

Communal Amenity Space

- 6.6.10 As noted above, a communal amenity space is proposed at the first and fifth floor levels, which would only be accessible by residents.

Outdoor Play Apace

- 6.6.11 The Council's Planning Obligations SPD, sets a quantitative standard of 4 square metres play space per child. When applied to the above child yield, this generates a requirement for 162sqm play space across all age groups. This includes 144sqm of play space for 0 -11 years.
- 6.6.12 As confirmed in the Landscape Strategy, the proposal would provide up to 167sqm of play space within the communal amenity, which would include play structures such as stepping stones, timber climbing polies, chess tables and a swing bench. It is considered that the proposed play space would provide sufficient play space for 0-11 years, but given the size of the communal amenity areas, it might not provide a sufficient play offering to older children. However, it is noted that there are parks within the surrounding area, including Harrow Recreation Ground, which would likely to be attractive to older children and those children seeking to play ball-sports. The GLA confirmed in their Stage 1 Response that where upgrades to off-site play areas are deemed appropriate, a suitable contribution towards these must be secured via planning conditions and the S106 Agreement. As noted above, a heads of term has been included to secure a financial contribution for outdoor play space. Discussions are ongoing and shall be finalised as part of the S106 negotiations.

Entrances and Approach

- 6.6.13 As noted above, all communal entrances would be clearly defined to strengthen the legibility of the key access points. All entrances would be afforded natural surveillance from the public highway and neighbouring properties. The proposals in this regard, are considered to be acceptable and the entrances would help define and activate the street frontages.

Shared Circulation

- 6.6.14 The SPG sets out a number of guidelines for shared circulation space, which includes the numbers of units that are accessed from each core (eight units); the provision of entry phone, or audio-visual verification to the access control system where applicable; natural light and adequate ventilation where possible; in schemes with more than eight storeys the provision of two lifts; and in the case of those buildings with wheelchair units the provision of more than one lift.
- 6.6.15 Each core would not serve more than eight flats per floor. Core A would be served by two lifts as it would have ten floors. Cores B and C would be served by a single lift. All of the wheelchair adaptable units would be located in cores A and C. Whilst the Cores would not have access to natural source of lighting, given the arrangement of the flats, the limited number of units accessed per floor and the a high proportion of dual aspect units, the lack of naturally light to the communal core lobbies is considered to be acceptable and is not uncommon.

6.6.16 Therefore, the proposed development is considered to provide an acceptable standard of shared circulation. A condition in relation to the provision of audio-visual verification to the access control system is considered to be appropriate.

Privacy and Outlook

6.6.17 The SPG calls for habitable rooms within dwellings to be provided with an adequate level of privacy in relation to neighbouring property, the street and other public spaces. Paragraph 2.3.36 of the SPG refers to yardstick separation distances of 18-21 metres between facing habitable room windows.

6.6.18 As a high density scheme there would inevitably be some overlooking relationships between homes within the development, particularly in the elevations facing towards to the first floor amenity area. The two parallel blocks located on either side of this amenity area are approximately 18m apart, which is in accordance with paragraph 2.3.36 of the SPG. These elevations contain habitable room windows and balconies, meaning that there would be a high level of visibility between homes on the same level (i.e. looking directly across) and perceptions of visibility to/from homes on other levels within the development. Given the high density nature of the proposal, which is consistent with the need to make effective use of this accessible previously-developed site, and the likely expectations of the future occupiers of such a development, this is not considered to be unacceptable. Future occupiers choosing to live at the development are likely to have different expectations about the level of privacy afforded from such a development than those choosing to live in more traditional, suburban environments. Furthermore, a number of units would have habitable room windows and balconies/terraces directly adjacent the communal amenity areas. The proposed Landscape Strategy has identified suitable boundary treatments and defensible planting to ensure a high standard of privacy for these units.

Daylight and Sunlight

6.6.19 This application was accompanied by a Daylight and Sunlight report. The daylight and sunlight analysis has been undertaken in two scenarios: firstly with the proposed scheme positioned in its true existing context and secondly, with the proposed scheme positioned in the indicative cumulative position with the indicative Tesco/Conran + Partners scheme included as built in the surrounding context, although it has not yet been the subject of a formal planning application.

6.6.20 For the purposes of measuring the performance of habitable rooms within the proposed development, the assessment uses the Average Daylight Factor. This method measures how much sky can be seen from the window and converts the results into a percentage of annual probable sunlight hours received. The BRE guidelines recommend that ADF values of 1% should be achieved in bedrooms, 1.5% in living rooms and 2 % in kitchens. In the case of combined living rooms and kitchen an ADF of 1.5% has been used. Where a room on a particular floor meets its target ADF value and the room above is of the same usage, dimensions and fenestration, the rooms above must also meet its target ADF value as it will have a similar or improved sky line when compared to the room below.

6.6.21 The report confirms the following for the true existing context:

- 144 out of 150 rooms which were analysed meet their target ADF values;
- A further 83 rooms will meet their target ADF values on the basis of the room below also meeting their target ADF values;
- 119 out of 150 rooms analysed achieve greater than 80% daylight distribution.
- Overall 227 out of 233 (97%) habitable rooms would meet their target ADF values.

6.6.22 The report confirms the following for the cumulative existing context:

- 135 out of 150 rooms which were analysed meet their target ADF values;
- A further 83 rooms will meet their target ADF values on the basis of the room below also meeting their target ADF values;
- 72 out of 124 rooms analysed achieve greater than 80% daylight distribution.
- Overall 218 out of 233 (94%) habitable rooms would meet their target ADF values.

6.6.23 Based on the above, the levels of daylight amenity for both scenarios will be very good.

6.6.24 With regards to sunlight, the BRE guidelines recommend that an outdoor amenity space receives at least 2 hours of sunlight on March 21st to at least 50% of its area. The Daylight and Sunlight Report confirms that on March 21st, the first floor amenity space receives two hours of sunlight to 91% of its area in both scenarios and the fifth floor amenity space receives 2 hours of sunlight to 92% of its area in both scenarios. Therefore, both amenity areas would receive sufficient sunlight, in both scenarios, in accordance with the BRE Guidelines recommendations.

6.6.25 In conclusion, officers consider that whilst clearly it is desirable for a new development to achieve 100% compliance with the recommendations of the BRE guidelines, it is inevitable that a site of the proposed density will require consideration of some compromise between daylight/sunlight, the provision of highly valued residential amenity space (including balconies) and other planning considerations that may influence the site layout and orientation of buildings. It should also be emphasised here that the recommended BRE guidelines for daylight and sunlight – whilst a valuable tool for measuring the degree of daylight and sunlight that would be achieved – do not form a part of the adopted development plan. Rather, Local Plan Policy DM1 requires a high standard of amenity and undertakes to have regard to a range of amenity considerations which includes, but is not limited to, the adequacy of light and outlook. While not all of the tested rooms/windows would achieve the recommended BRE standards, it is demonstrated that the majority would. Furthermore, the majority of flats would benefit from a dual aspect, and all flats would meet or exceed the London Plan minimum space standards, and have access to private amenity space. On balance, therefore, it is considered that the poor performance of some parts of the development in terms of the recommended BRE guidelines is not unacceptable.

6.6.26 Subject to the above conditions, the proposed development would provide an acceptable standard of residential accommodation for future residents, in accordance with policy 3.5C of The London, policies DM1 and DM27 of the Harrow Development Management Policies (2013), the Supplementary Planning Document - Residential Design Guide (2010), The London Plan Housing Supplementary Planning Guidance (2016) and the Technical Housing Standard (2015)

6.7 Impact to Residential Amenities

6.7.1 The proposed units facing towards High Mead would have a separation distance of approximately 11.5m from the flank wall of No.73 Station Road which is a pharmacy with residential accommodation above, and would have a separation distance of approximately 20m from the front elevation of Axis Court, a three storey residential building. Nos 1-24 High Mead is located to the rear of the site, opposite Tesco Superstore, and would have a separation distance of approximately 21m from the taller element of the proposed building. The western side of Station Road opposite the application site is characterised by residential properties, including dwellinghouses and flats. They would be located approximately 33m from the proposed development, with Station Road intervening. It is considered that due to the relationship between the proposed development and neighbouring properties, it would not result in a detrimental impact to the residential properties on High Mead and Station Road.

6.7.2 Dominion Parade is located to the south of the application site. The taller element of the proposed development would be located approximately 11m to the rear of Dominion Parade, however any views from these windows to the rear windows of Dominion Parade would be at an oblique angle. The proposed first floor flat (C1.01) and second floor flat (C2.01) would be located directly adjacent to Dominion Parade, and both flats have a balcony. During the course of the application, the applicant submitted amended plans to ensure an obscured glazed panel is installed along the southern side of these balconies, to prevent any direct overlooking from the balconies of those units into habitable room windows on the rear of Dominion Parade. The southern edge of the first floor amenity area would have a metal balustrade. Details in relation to soft landscape and boundary treatments would be secured by a planning condition. Through a suitable boundary treatment with planting to provide screening, it is considered that the first floor communal area would not result in a detrimental impact to the residential amenities of Dominion Parade.

6.7.3 As noted above, this application was accompanied by a Daylight and Sunlight report and the analysis has been undertaken in two scenarios: existing and cumulative. The report also assessed the impact of the proposed development of the daylight and sunlight of adjacent properties. With regards to 1-5 High Mead located to the north of the application site, all windows assessed were found to meet the BRE Guidelines for target values for daylight and sunlight. The report also found that given the location of 1-5 High Mead in relation to the Tesco site and the site being directly opposite Tesco and offset from the application site, it concluded that the majority of the impact to 1-5 High Mead in the cumulative scenario arises from the Tesco/Notting Hill Genesis scheme. However, this

scheme has not yet been the subject of a formal planning application. With regards to 1-16 Dominion Parade the report states in daylight terms, when comparing the existing and proposed scenario, 24 out of 25 windows meet the BRE Guidelines target values for VSC, whilst all rooms experience less than 20% reduction in daylight distribution. In sunlight terms, all windows which face within 90 degrees of due south meet the BRE guidelines and winter APSH with the proposal in place. Isobel House is a three storey building (with habitable rooms in the roofspace) on Station Road opposite the application site. The report concludes that when comparing the existing and proposed scenarios, all windows meet the BRE Guidelines target values for VSC, whilst all rooms experience less than 20% reductions in daylight distribution.

- 6.7.4 The proposed development, due to its layout, form and height, including the set back of the taller element from nearby residential properties and its relationship with neighbouring properties in terms of orientation, would not result in a detrimental impact to the residential amenities of neighbouring properties. The proposed development therefore complies with the National Planning Policy Framework (2019), policy 7.6B of The London Plan (2016), policy D1 of the Draft London Plan (2017), Policy DM1 of the Harrow Development Management Policies Local Plan (2013) and the adopted Supplementary Planning Document Residential Design Guide (2010).

6.8 Housing Provision and Affordable Housing

- 6.8.1 Given the nature of the residential use of the site, and the proposed quantum of units, the application is liable to provide an affordable housing contribution. In support of the planning application, the applicant has submitted a Financial Viability Assessment, which attempts to demonstrate that the proposed development is unable to provide a policy compliant scheme in terms of affordable housing provision. The appraisal concludes that the land value return is below the benchmark land value and thus would be deemed unviable. However, despite this, the applicant is committed to bringing the scheme forward and has confirmed the following

“Our client hopes (and expects) that the market will improve and that the sale values of the 78 apartments will increase. In theory, the developer could be completing sales in up to 5 years of a grant of planning permission. In general, property values tend to go in cycles and, although future trends in the property market are difficult to predict, it would be reasonable to assume some improvement in values by then”.

- 6.8.2 The scheme as submitted, proposed the provision of 9 affordable units located within Core B of the proposed development. The affordable housing was proposed as intermediate housing for shared ownership, comprising 11.5% of the total number of units. The applicant submitted a financial viability appraisal (FVA) which stated *“a policy compliant alternative of the 9 apartments as a 60%/40% tenure mix of London Affordable Rent and Intermediate Housing has been considered but would render the project significantly less viable because of the relatively low RP payment for LAR compared with shared ownership”.*

- 6.8.3 The Councils preferred tenure split is 60% London Affordable Rent and 40% Intermediate Housing. Therefore, an affordable housing provision of 11.5% comprising 100% Intermediate Housing is not policy compliant and does not comply with the Councils preferred tenure split.
- 6.8.4 The Council has tendered an external review of the applicant's FVA and a full report (prepared by Savills) on the assumptions used including the independent assessment on the applicant's FVA conclusion is currently in a draft format. However, Savills have concluded that many of the Applicant's assumptions are reasonable, that the scheme is technically unviable and that the current affordable housing offer represents the maximum amount of affordable housing which may be reasonably delivered, in addition to the additional planning obligations. However, a 100% intermediate housing scheme would not comply with the local requirements for affordable housing, which has an emphasis on the provision of London Affordable Rent. Furthermore, the Housing Enabling Officer raised concerns on whether the low number of affordable units proposed would be of a commercial interest to a Registered Provider. Therefore in this case, it is considered that a cash in lieu of provision contribution would be appropriate. As such, Savills were requested to calculate the commuted sum based on the difference in the GDV of the 9 LS-Own units and a 100% market unit scheme, in accordance with paragraph 2.62 of the Mayor's SPG. The commuted sum is calculated at £1,260,000 which the applicant has confirmed. Discussions are ongoing with regards to the late stage review cap and shall be finalised as part of the S106 negotiations.
- 6.8.5 It is considered that the remaining assumptions made within the Financial Viability Assessment are generally reasonable and as such accepted. Subject to a planning obligation, it is considered that the proposed development would deliver the maximum reasonable amount of affordable housing and would be acceptable in terms of the Planning Obligations SPD (2013).
- 6.8.6 In conclusion, notwithstanding the outstanding matters relating to the early stage review and the late stage review cap, the proposed development complies with the National Planning Policy Framework, policies 3.11 and 3.12.A/B of The London Plan (2016), policies H5 and H8 of The Draft London Plan (2019), policy CS1.J of the Harrow Core Strategy (2012), policy AAP13 of the Harrow and Wealdstone Area Action Plan (2013) and the Mayor's SPG.

6.9 Housing Density and Unit Mix

Density

- 6.9.1 Table 3.2 of The London Plan (2016) sets out that a density range of 215-405 units per hectare and 650-1100 habitable rooms per hectare would be most appropriate for this site. The development proposes a density of 198 u/ha which has a slight shortfall from the density range set out in The London Plan (2016) and proposed 960 hr/ha, which exceeds the density range.
- 6.9.2 However, whilst the density range for the number of habitable rooms per hectare exceeds the current London Plan standards, the emerging London Plan removes

the requirement to base densities on a density matrix, but requires the local plan to adopt a site specific density assessment based on optimising development potential within sustainable locations with a PTAL range of 3-6. In view of this, it is considered that the density of development would achieve the aspirations of the emerging London Plan and would seek to optimise the development potential on this site. Furthermore, given its proximity to the metropolitan centre and underground/rail station, it is considered that the density of development proposed on this site can be supported and overall consider that the proposals would meet the aspirations of the current and emerging development plans.

Unit Mix

- 6.9.3 The development would largely provide one-bed and two-bed units, with a small proportion of three-bed units as set out in the table below:

Studios	1 Bed	2 Bed	3 Bed	Total
3	17	43	15	78

- 6.9.4 For a scheme of this scale and location which is likely to be attractive to small family or professional groups, it is considered that the units would be appropriate and attractive.
- 6.9.5 In conclusion, the proposed development complies with the National Planning Policy Framework, policies 3.4, 3.8 and 3.9 of The London Plan (2016), policy H2 of The Draft London Plan (2019), policy CS1.1 of the Harrow Core Strategy (2012) and policy AAP13 of the Harrow and Wealdstone Area Action Plan (2013).

6.10 Traffic, Safety and Parking

- 6.10.1 The application site is located within the Harrow Metropolitan Centre and has a PTAL rating of 5. There are local convenience stores, supermarkets salons, restaurants, pharmacies etc in close proximity. Furthermore, the site is within walking distance of St Anns and St Georges shopping centres and Harrow on the Hill and Harrow & Wealdstone Stations. There is a bus stop located immediately to the front of the application site, providing frequent and reliable services to the surrounding area including local rail stations. The application site is therefore within a highly sustainable location. The application was accompanied by a Transport Assessment and a Travel Plan.

Servicing

- 6.10.2 The Applicant explored several strategies for the servicing of the development, including servicing internally, to the rear of the site. However, a Road Safety Audit demonstrated that this would cause a conflict with the proposed accessible parking spaces. An amended Transport Assessment was submitted which demonstrated that there would be sufficient capacity to provide an on-street loading bay along High Mead. The Highways Department was consulted who raised no objection to the proposed development. It is noted that the TfL and GLA raised concerns with regards to the proposed loading bay located on High Mead. An amended Transport Assessment and an Explanatory Note was submitted to

the TfL for their review, which provides further details and justification for the proposed servicing arrangements. At the time of writing this report, comments from TfL were pending. Therefore, an Addendum will be issued to provide further details in this regard. Notwithstanding the concerns raised by TfL, the Highways Department have raised no objection to the proposed loading bay on High Mead as they are satisfied that there is sufficient capacity. However, this is on the basis that further details in relation to deliveries and servicing, and the on-going review and monitoring of such arrangements are secured within the S106 Agreement.

Car Parking/Traffic/Highway Safety

- 6.10.3 There are no existing on-site car parking spaces provided for patrons or staff of the existing cinema and church. For those staff and patrons travelling to Safari Cinema by car would therefore need to rely on on-street parking or nearby car parks. Furthermore, there is no provision for secured cycle parking nor are there cycling facilities such as showers or lockers.
- 6.10.4 The applicant conducted a parking survey which demonstrated spare on-street parking capacity within 200m of the application site, apart from a Friday lunchtime and on a Sunday. It was observed that High Mead is heavily parked on a Sunday during the peak time of the church. It is noted that the proposed development would result in the loss of the existing church and a reduction in cinema seats, from 745 to approximately 286. Overall, there would be a reduction in community floorspace from 3,100 sqm to 1,462 sqm. Therefore, it is considered that the proposed development would result in an overall reduction in the demand for on-street car parking.
- 6.10.5 The Draft London Plan (2019) confirms that new developments located within an area with a PTAL rating of 5 should be car-free. However, in order to ensure genuine housing choice, disabled person's parking should be provided for new residential developments. In accordance with policy T6.1(G) of The Draft London Plan (2019). At least one disabled space should be provided for 3% of the dwellings. Therefore, in accordance with policy T6.1(G) three accessible spaces are proposed to the rear of the building, all of which would have access to an electric charging point. The Transport Assessment confirms that a further 5 disabled spaces would be provided on-street (if and when required) to meet the 10% provision for residential development. TfL raised concerns regarding the distance between the proposed disabled spaces and the pedestrian entrance on Station Road. However, the Highways Department have confirmed that they are satisfied that there is sufficient space on the public highway to provide an additional five disabled spaces. The existing art-deco façade which fronts Station Road is locally listed and would be revealed and restored as a result of the proposed development. The section of Station Road immediately opposite the application site has a bus-stop and double yellow lines. Therefore, due to the existing site circumstances it is not possible to provide an accessible parking space to the front of the building. Any on-street parking would need to be provided on High Mead. Furthermore, TfL requested a financial contribution to fund the provision of electric vehicle charging points for these additional 5 disabled spaces. However the provision of such spaces would be demand-led and therefore, on this basis it is considered inappropriate to request a financial contribution towards the

provision of ECVP for the five disabled spaces, which may or may not be provided as they are not necessary to make the development acceptable in planning terms, and therefore to request such a contribution would be contrary to Section 122 (2a) of The Community Infrastructure Levy Regulations 2010. It is noted that the TfL raised concerns with regards to the proposed location of the additional accessible parking spaces for both the residential and non-residential uses on the public highway and the lack of a direct access from the proposed accessible parking spaces to the residential entrance lobbies. An amended Transport Assessment and an Explanatory Note was submitted to the TfL for their review, which provides further details and justification for the location of the proposed accessible parking. At the time of writing this report, comments from TfL were pending. Therefore, an Addendum will be issued to provide further details in this regard. Notwithstanding the concerns raised by TfL, as noted above, the Highways Department have not raised an objection in this regard, subject to the submission of a Parking Design and Management Plan, which would be secured by a condition.

- 6.10.6 Given the scale of the development, it is deemed necessary to recommend a condition to ensure that prior to commencement of work, a construction method statement and logistics plans are submitted and approved to ensure that there is no impact on the public highway.

Cycle Parking

- 6.10.7 A total of 160 cycle parking spaces are proposed, including long and short stay spaces for residential uses, as well as staff and visitor parking. This comprises:
- Residential: 145 long stay spaces and 2 short stay spaces.
 - Non-Residential: 3 long stay spaces and 10 short stay spaces.
- 6.10.8 As confirmed by TfL, the proposed number of cycle parking spaces complies with The Draft London Plan (2019) requirements.
- 6.10.9 All residential cycle parking would consist of a two-tier stands located within cycle stores on the basement and ground floor. TfL advised that two-tier stands are not suitable for all users and all types of cycles and the provision of other types of stand should be considered. Furthermore, it is proposed to locate 5 Sheffield stands for cinema visitors adjacent to the Entrance C Lobby, which may cause an obstruction to refuse collectors accessing the refuse store. It is therefore considered appropriate to include a condition which requires details of cycle parking, including their location and types of stands to be submitted to the LPA for approval. It is noted that the TfL raised concerns with regards to the proposed location of the short-stay cycle stands and the type of stands proposed. However, the LPA is satisfied that these issues can be addressed by such a condition.
- 6.10.10 Overall, subject to the above conditions and in accordance with the advice received from the Highways Department, it is considered that the proposed servicing arrangements are acceptable and the proposal would not have an adverse impact on the free flow of traffic or highway and pedestrian safety. Furthermore sufficient cycle parking and accessible parking spaces would be provided for future occupiers and visitors. The proposed development therefore complies with the National Planning Policy Framework (2019), policies 6.3, 6.9

and 6.13 of The London Plan (2016), policies T3, T4, T5 and T6 of The Draft London Plan (2019), policy CS1.R of the Core Strategy (2012) and policies DM1, DM42, DM43, DM44 and DM45 of the Harrow Development Management Local Plan (2013).

6.11 Accessibility

- 6.11.1 The Design and Access Statement has confirmed all of the proposed flats would comply with Part M and 10% of them would be wheelchair adaptable. While the above compliance with Part M of the Building Regulations is acknowledged, a condition of approval will ensure that the proposed development would meet regulation M4 (2) of the building Regulations which would secure an appropriate standard for future occupiers and make the units accessible to all.
- 6.11.2 Three disabled parking bays are proposed, in accordance with policy T6.1(G) of The Draft London Plan (2019). Whilst it is noted that the proposed spaces are to be located to the rear of the unit, it is considered that this is the most appropriate location, in order to maintain the architectural integrity of the building and the locally listed façade. A Transport Assessment was submitted which as noted above, clarifies that the additional five spaces would be provided on the local highway, when the Highways Department considers there is a demand for such additional provision.
- 6.11.3 Accordingly, subject to the above condition, it is considered that the proposed accommodation would be satisfactory and as such would comply with the National Planning Policy Framework (2019), policies 3.5 and 3.8 of the London Plan (2016), policies D3 and D5 of The Draft London Plan (2019), policy CS1.K of the Core Strategy (2012) and policies DM1 and DM2 of the Harrow Development Management Local Plan (2013).

6.12 Energy and Sustainability

- 6.12.1 The application is accompanied by an Energy & Sustainability Statement which outlines a number of sustainability and carbon reduction proposals and is broadly considered to follow GLA guidance for such statements. In reducing carbon emissions, the energy statement follows the energy hierarchy by seeking to use less energy in the first instance (be lean), supplying energy efficiently (be clean) and finally using renewable energy (be green).
- 6.12.2 The total carbon reductions achieved on-site through the above measures are 34.0% (residential) and 69% (non-residential). The over-achievement for the non-residential element reduces the offset contribution required for the residential element to achieve zero carbon; such an approach is considered to be acceptable.
- 6.12.3 An offset contribution will be required to fund carbon reduction measures elsewhere in the borough. This equates to a contribution of £48,540. Provided this contribution is secured through a section 106 contribution, the energy strategy is considered to meet relevant London Plan policies. In addition a condition requiring that the development is constructed and operated generally in accordance with the energy strategy submitted with the application has been added.

- 6.12.4 In accordance with the advice received from the GLA in their Stage 1 response, the Applicant has submitted a breakdown of the residential water consumption. The submitted details confirms that the total water consumption would be approximately 102 litres per head per day, in accordance with the target provided in policy 5.15 of The London Plan (2016) and SI5 of The Draft London Plan (2019). However, it is considered appropriate to include a condition which requires a water strategy to be submitted to the Local Planning Authority, which demonstrates an efficient use of mains water.
- 6.12.5 Subject to the above conditions, the proposed development complies with the National Planning Policy Framework (2019), policies 5.2, 5.3 and 5.15 of The London Plan (2016), policies SI2 and SI5 of The Draft London Plan (2019), policy CS1.T of the Core Strategy (2012) and policies DM1, DM12 and DM14 of the Harrow Development Management Local Plan (2013).

6.13 Contamination and Flood Risk

- 6.13.1 The application site is located within a critical drainage area. The Council's Drainage Department was consulted who raised no objection to the proposed development, subject to conditions in relation to the disposal and attenuation of foul and surface disposal. These conditions have been added accordingly.
- 6.13.2 The drainage engineer also states that permeable materials should be used for all paving and hardstanding. A suitable condition has been attached to this effect.
- 6.13.3 The application was not accompanied by a Contamination Report. The Environmental Health Department confirmed that they are not aware of any potential for contaminants. However, the Environmental Health Department raised no objection to the proposed development in this regard, subject to a condition which would require a desk-top study to be carried out by a competent person, a site investigation and a written method statement to address
- 6.13.4 Subject to the above conditions, the proposed development complies with the National Planning Policy Framework (2019), policies 5.13 and 5.14 of The London Plan (2016), policy SI13 of The Draft London Plan (2019) policy CS1.W of the Core Strategy (2012) and policies DM1 and DM10 of the Harrow Development Management Local Plan (2013).

6.14 Air Quality

- 6.14.1 Policy 7.14B of the London Plan seeks to minimise exposure to existing poor air quality and make provision to address local problem of air quality. It goes onto stated *inter alia* measures to reduce emissions during demolition and construction; proposals to be 'air quality neutral' and not to lead to further deterioration in air quality; ensure on-site provision of measures to reduce emissions; and assessment of the air quality implications of biomass boilers. Policy DM1 (D.h) of the DMP also reinforces the view of assessing the impact of proposal on *inter alia* vibration, duct and air quality.

- 6.14.2 This application is supported with an Air Quality Assessment (AQA) which assesses the impact of the proposed development both during construction period and operational period on the air quality in the surrounding area.
- 6.14.3 Harrow is within an 'Air Quality Management Area (AQMA) due to the exceedance of the annual and hourly mean Nitrogen Dioxide and the 24 hours mean small airborne particles.
- 6.14.4 The AQA concludes that construction works have the potential to create dust and particulate matter during construction works. The report recommends that through good site practice and the implementation of suitable mitigation measures, the impact of such releases may be effectively mitigation and the resultant impacts are considered to be negligible. With sufficient mitigation in place, the construction impacts are judged as being not significant.
- 6.14.5 Modelling was carried out to assess the likely exposure of future residents and the suitability of the site for its proposed end use with regards to local air quality. The report concludes that the predicted concentration of relevant pollutants will be below the relevant air quality objectives/standards and therefore future residents would not be exposed to an unacceptable level of pollutant concentrations. No further mitigation is required in this regard.
- 6.14.6 Overall, there are no air quality constraints arising from the proposed development.
- 6.14.7 Whilst the Council's Environmental Health Team have not commented in respect of this application, it is considered that the mitigations suggested in the AQA in respect of the construction phase would be adequately captured with the Construction Logistics condition. Subject to the imposition of the recommended condition, the proposed development would give rise to no conflict with the above stated policies.

6.15 Statement of Community Involvement

- 6.15.1 The NPPF, Localism Act and the Council's Statement of Community Involvement encourage developers, in the cause of major applications such as this, to undertake public consultation exercise prior to submission of a formal application.
- 6.15.2 Prior to the submission of this application, the applicant held a public consultation event. The applicant sent letters to local residents that residing close to the site, local stakeholders and ward councillors to invite them to attend the public consultation event.
- 6.15.3 Following the validation of the planning application, the Council sent letters of consultation to local residents in the surrounding area inviting them to make representations on the proposed development. In addition, site notices were displayed and the application was advertised in the local newspaper.
- 6.15.4 The applicant has sought to encourage public consultation in respect the proposal in line with the guidance set out in the NPPF and the Localism Act.

6.16 S.106 Obligations and Infrastructure

6.16.1 The heads of terms of the section 106 agreement have been set out above. These are considered necessary to make the application acceptable, in accordance with policy 3.2 of The London Plan 2016, policies CS1.Z/AA and CS2.Q of the Harrow Core Strategy (2012) and policy DM50 of the Harrow Development Management Local Plan (2013).

6.17 CONCLUSION AND REASONS FOR APPROVAL

6.17.1 The proposed redevelopment of the site would provide a high quality development comprising of a community use on ground floor and residential accommodation at upper levels. The proposed internal layouts, specifications and services offered by the proposed development would provide an acceptable standard of accommodation.

6.17.2 The proposal would enhance the urban environment in terms of material presence, attractive streetscape and would make a positive contribution to the local area, through the restoration of the art deco, locally listed façade.

6.17.3 The layout and orientation of the buildings and separation distances to neighbouring properties are considered to be satisfactory to protect the amenities of the neighbouring occupiers.

6.17.4 The decision to GRANT planning permission has been taken having regard to the National Planning Policy Framework 2019, the policies and proposals in The London Plan 2016, The Draft London Plan (2019), the Harrow Core Strategy 2012 and the Development Management Policies Local Plan 2013, and to all relevant material considerations, and any comments received in response to publicity and consultation.

APPENDIX 1: Conditions and Informatives

Conditions

1. Time Limit

The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

REASON: To comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. Approved Drawing and Documents

Save where varied by other planning conditions comprising this planning permission or unless otherwise agreed in writing by the local planning authority, the development hereby permitted shall be carried out in accordance with the following approved plans to show the redevelopment of the site to include the erection of a part 11/part 5 storey building with basement level comprising of 78 residential units (3 X studios) (17 X 1 bed) (43 X 2 bed) (15 X 3 bed); cinema (Use class D2) to ground floor and ancillary cafe to first and second floors; parking; landscaping; bin and cycle stores

Approved Plan:

IF18-04 001; IF18-04 002A; IF18-04 003; IF18-04 010B; IF18-04 100B; IF18-04 101A; IF18-04 102A; IF18-04 103A; IF18-04 104; IF18-04 105; IF18-04 106; IF18-04 107; IF18-04 108; IF18-04 109; IF18-04 110; IF18-04 111; IF18-04 200; IF18-04 201A; IF18-04 202A; IF18-04 203; IF18-04 204; IF18-04 300A; IF18-04 301A; IF18-04 302; IF18-04 303; IF18-04 400; IF18-04 401; IF18-04 402; IF18-04 403; IF18-04 404; IF18-04 405; IF18-04 406; IF18-04 407; IF18-04 408

Approved Documents:

Air Quality Assessment (Entran Environmental & Transportation); Biodiversity (Greengage Environmental Limited); Construction Logistics Plan (Yes Engineering Group Ltd); Crittal Window Report (Crittal Windows Ltd); Design and Access Statement (If Architecture); Daylight & Sunlight Report (Waldrams Ltd); Delivery and Servicing Plan (Yes Engineering Group Ltd); Economic Statement (WSP Indigo Planning); Energy and Sustainability Statement (JS Lewis Ltd); Equality Impact Assessment (WSP Indigo Planning); Façade Condition Report (Calford Seaden); Supplementary Façade Condition Report (Calford Seaden); Façade Investigation Report (Calford Seaden); Heritage Statement (Prentice Moore Heritage Consultants); Landscaping Statement (Macfarlane Associates); Environmental Noise Assessment (Entran Environmental & Transportation); Updated Environmental Noise Assessment (Entran Environmental & Transportation); Planning Statement (Maddox

Associates); Statement of Community Involvement (Maddox Associates); SUDS and Drainage Strategy (RMA Environmental); Transport Assessment (Yes Engineering Group Ltd); Travel Plan (Residential) (Yes Engineering Group Ltd); Travel Plan (Community use) (Yes Engineering Group Ltd); ATZ Survey (Yes Engineering Group Ltd); Viability Assessment (HEDC Ltd); Views Assessment (Influence Environmental Ltd); Existing and proposed plans (If Architecture)

REASON: For the avoidance of doubt and in the interests of proper planning.

3. Demolition – PRECOMMENCEMENT CONDITION

The works of demolition or alteration by way of partial demolition hereby approved shall not be commenced before contract(s) for the carrying out of the completion of the entire scheme of works for which consent is hereby granted, including the works contract, have been made and evidence of such contract(s) have been submitted to and approved in writing by the Local Planning Authority.

REASON: to ensure that the site is redeveloped immediately following demolition and to ensure that premature demolition does not take place and that an unsightly gap or derelict site does not detract from the character and appearance of the area, in accordance with policies 7.4 of the London Plan (2016) and policy DM 1 of the Harrow Development Management Policies Local Plan (2013).

4. Construction Logistics Plan - PRECOMMENCEMENT CONDITION

No development shall take place, including any works of demolition, until a Detailed Construction Logistics Plan has been submitted to, and approved in writing by, the local planning authority in accordance with the format and guidance provided by the Transport for London – www.constructionlogistics.org.

The Detailed Construction Logistics Plan shall provide for:

- (a) Parking of vehicles of site operatives/visitors;
- (b) HGV access to site – loading and unloading of plant and materials;
- (c) Number of HGV's anticipated;
- (d) Storage of plant and materials used in constructing the development;
- (e) Programme of work and phasing;
- (f) Site layout plan;
- (g) Highway condition (before, during, after);
- (h) Measures to control dust, vibration and dirt during demolition, earthworks and construction;
- (i) A scheme for recycling/disposing of waste resulting from demolition, earthworks and construction works;
- (j) Details showing the frontage/ the boundary of the site enclosed by site

- hoarding to a minimum height of 2 metres;
- (k) Details of cranes and other tall construction equipment (including obstacle lighting).

The development shall be carried out in accordance with the approved Detailed Construction Logistics Plan, or any amendment or variation to it as may be agreed in writing by the local planning authority.

REASON: To minimise the impacts of construction upon the amenities of neighbouring occupiers and to ensure that development does not adversely affect safety on the transport network in accordance with policy 6.3 of The London Plan (2016) and policies DM1 and DM43 of the Harrow Development Management Policies Local Plan (2013).

5. Recording - PRECOMMENCEMENT CONDITION

The works of demolition or alteration by way of partial demolition hereby approved shall not commence until a full survey of the building (interior and exterior), to a level 4 in accordance with Historic England's guidance document entitled 'Understanding Historic Buildings: A Guide to Good Recording Practice', shall be submitted to and approved in writing by the Local Planning Authority.

If at any stage any further historic fabric, elements or details are uncovered, the survey shall be updated and submitted to and approved in writing by the Local Planning Authority within one month of the completion of the demolition works.

The final approved survey shall be submitted to Historic England's Greater London Historic Environment Record (GLHER) and the Local Planning Authority notified of this within two weeks of their approval of the final survey.

REASON: To protect the special architectural or historic interest of the locally listed building in accordance with policies 7.6 and 7.7 of the London Plan (2016) and policies DM1 and DM7 of the Harrow Development Management Policies Local Plan (2013).

6. Levels - PRECOMMENCEMENT CONDITION

No site works or development shall commence until details of the levels of the building(s), road(s) and footpath(s) in relation to the adjoining land and road/highway(s), and any other changes proposed in the level of the site, have been submitted to, and agreed in writing by the local planning authority. The development shall be carried out in accordance with the details so agreed.

REASON: To ensure that the works are carried out at suitable levels in relation to the highway and adjoining properties in the interests of the amenity of neighbouring residents, the appearance of the development, drainage, gradient of access and future highway improvement in accordance with policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

7. Contamination - PRECOMMENCEMENT CONDITION

No demolition shall take place until a scheme ('the first scheme') for identifying, managing and disposing of any potential contamination hazards found during demolition of the existing buildings and structures on the site has first been submitted to, and agreed in writing by, the local planning authority. No development other than demolition shall take place until a scheme ('the second scheme') for the management of contamination risk at the site has first been submitted to, and agreed in writing by, the local planning authority. The second scheme shall include the following:

- (a) details of a site investigation to provide information for a detailed assessment of the risks to all receptors that may be affected, including those off site;
- (b) the results of the site investigation and an options appraisal and remediation strategy giving full details of remediation measures and how they are to be undertaken; and
- (c) a verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy are complete and identifying any requirements for longer-term monitoring of pollutant leakages, maintenance and arrangements for contingency action.

The demolition shall be carried out in accordance with the first scheme so agreed. The development other than demolition shall be carried out in accordance with the second scheme so agreed.

REASON: To ensure that the development does not activate or spread potential contamination at the site and that the land is appropriately remediated for the approved uses, in accordance with Policy 5.21 of the London Plan (2015) and Policy DM15 of the Local Plan (2013). To ensure that measures are agreed and in place to identify and manage potential sources of contamination during the demolition and construction phases of the development, this condition is a PRE-COMMENCEMENT condition.

8. Foul Disposal

The development other than demolition works hereby permitted shall not commence until details of the works for the disposal of sewage to be provided on site have been submitted to, and agreed in writing by, the Local Planning Authority. The development shall be carried out in accordance with the works so agreed and the works shall thereafter be retained.

REASON: To ensure that there is adequate waste water infrastructure in place to serve the development and to ensure the separation of surface and foul water systems, in accordance with policy 5.14 B of The London Plan (2016) and policy DM10 of the Development Management Policies Local Plan (2013).

9. Surface Water Attenuation, Storage and Disposal

The development other than demolition works hereby permitted shall not commence until details for the works for the attenuation, storage and disposal of surface water to be provided on site have been submitted to, and agreed in writing by, the Local Planning Authority. The development shall be carried out in accordance with the works so agreed and the works shall thereafter be retained.

REASON: To ensure that the development incorporates sustainable drainage systems and achieves greenfield run-off rates, and to ensure the separation of surface and foul water systems, in accordance with policy 5.13A of The London Plan (2016) and policy DM10 of the Harrow Development Management Policies Local Plan (2013).

10. Fire Strategy

The development hereby approved shall not progress beyond damp proof course level until a Fire Statement produced by a third party suitably qualified assessor has been submitted to, and agreed in writing by, the local planning authority. The development shall be carried out in accordance with the strategy so agreed and shall be retained as such thereafter unless otherwise agreed by the local planning authority or Fire Authority.

REASON: To ensure that the development proposals achieve the highest standard of fire safety.

11. Basement Protection

Notwithstanding the approved plans, the development other than demolition works, hereby permitted shall not commence until details for basement protection for ground water flooding, which would include waterproofing and water tanking details, have been submitted and approved by the Local Planning Authority. The development shall be completed in accordance with the approved details and shall thereafter be retained.

REASON: To build in resistance and resilience in managing, reducing and mitigating the effects of flood risk in accordance with policy SI13 of The Draft London Plan (2019) and policies DM1 and DM10 of the Harrow Development Management Local Policies Plan (2013).

12. Permeable Paving

The development other than demolition works hereby permitted shall not commence until full details of the permeable paving and details relating to the long term maintenance and management of the on-site drainage are submitted to and approved in writing by the Local Planning Authority. Details thereby approved shall be retained thereafter.

REASON: To ensure that the development has adequate drainage facilities, to reduce and mitigate the effects of flood risk and would not impact the character and appearance of the development, in accordance with policy 5.13 of The London Plan (2016) and policies DM1 and DM10 of the Harrow Development Management Local Policies Plan (2013).

13. Noise and Vibration

The development other than demolition works hereby permitted shall not commence until a scheme which specifies the provision to be made for the control of noise (including noise from people emanating from the site) and vibration from the hereby approved cinema and café on the flats above and the neighbouring properties, has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include such combination of physical, administrative measures, noise and vibration limits and other measures as may be approved by the Local Planning Authority. Thereafter, the scheme shall be installed and shall be operational prior to the first occupation of the flats, or the commencement of the use of the cinema, whichever is the soonest, and shall be maintained in full compliance with the approved details and shall be thereafter retained, unless otherwise agreed in writing by the Local Planning Authority.

REASON: In order to safeguard the amenities of neighbouring residents in accordance with policy 7.15 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

14. Openings

The development other than demolition works hereby permitted shall not commence until the following details have been submitted to and approved in writing by the Local Planning Authority:

- Existing elevations and sections (to a scale of 1:10 or 1:20) of all windows and doors in the locally listed façade;
- Proposed elevations and sections (to a scale of 1:10 or 1:20) of all windows and doors in all elevations, including that proposed for the retained facade.
- A 1:1 sample mock-up of a non-façade window opening with surrounding brickwork and stone;
- A 1:1 sample mock-up of a façade window opening with surrounding render.

The development shall be carried out in accordance with the details, samples and drawings so agreed and shall be retained as such thereafter.

REASON: To ensure that the development is carried out to the highest standards of architecture and materials, in accordance with policy 7.6 of the

London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

15. Materials

Notwithstanding the details shown on the approved drawings, the development shall not progress beyond damp proof course level until samples of the materials (or appropriate specification) to be used in the construction of the external surfaces noted below have been submitted to the Local Planning Authority to be agreed in writing,:

- (a) facing materials for the buildings, including roof;
- (b) windows;
- (c) doors;
- (d) balconies and terraces including privacy screens;
- (e) decking;
- (f) louvers;
- (g) boundary treatment including all pedestrian/ access gates;
- (h) ground surfacing.

The development shall be carried out in accordance with the details so agreed and shall be retained as such thereafter.

REASON: To ensure that the development is carried out to the highest standards of architecture and materials in accordance with policies 7.6 and 7.7 of the London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

16. Fixtures/Gates/Downpipes etc

The development hereby approved shall not progress beyond damp proof course level until full details, including specifications have been submitted to and approved in writing by the local planning authority:

- a) Rainwater disposal systems (including downpipes) and soil stacks;
- b) Rollershutters, gates and other means of controlling access to the car park;
- c) Security alarms and CCTV.

The development shall be carried out in accordance with the approved details.

REASON: To ensure a high quality of design in accordance with policy 7.4.B of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

17. External Lighting

Notwithstanding the approved details, the development hereby permitted shall not progress beyond damp proof course level, until details of the lighting of all public realm and other external areas (including buildings) within the site has been submitted to the Local Planning Authority. This includes all details of the

lighting including sourcing/ manufacturer details, specification, elevations, light spillage and lighting levels The development shall be implemented in accordance with the approved details and shall be retained as such thereafter.

REASON: To ensure that the development incorporates lighting that contributes to Secured by Design principles, achieves a high standard of quality in accordance with policies DM1 and DM2 of the Harrow Development Management Policies Local Plan (2013) and to ensure that the development does not unduly impact on the biodiversity potential of the site in accordance with policy DM20 of the Harrow Development Management Policies Local Plan (2013).

18. Water Consumption

The development hereby approved shall not progress beyond damp proof course level until a strategy for the efficient use of mains water within the residential parts of the development, pursuant to a water consumption limit of 105 litres per person per day, has been submitted to, and agreed in writing by, the Local Planning Authority. The development shall be carried out in accordance with the strategy so agreed and shall be retained as such thereafter.

REASON: To ensure that the development makes efficient use of mains water in accordance with policy 5.15 of The London Plan (2016) and policy DM12 of the Harrow Development Management Polices Local Plan (2013).

19. Boundary Treatment

Notwithstanding the approved details, the development hereby permitted shall not progress beyond damp proof course level, until a plan indicating the positions, design, materials and type of boundary treatment/screening to be erected has been submitted to, and approved in writing by, the local planning authority. The development shall be completed in accordance with the approved details prior to occupation, and shall thereafter be retained.

REASON: To safeguard the amenity of neighbouring residents and the character of the locality in accordance with policies 7.4 and 7.6 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

20. Landscape Masterplan

Notwithstanding the approved details, the development hereby permitted shall not progress beyond basement level, until a Landscape Masterplan for the site has been submitted to, and approved in writing by the Local Planning Authority which shall include:

- a) All hard and soft landscape details, including details of the planting, hard

surfacing materials including decking, raised planters, watering and drainage and external seating. The hard and soft landscaping details shall demonstrate how they would contribute to privacy between the approved private terraces and the communal pedestrian routes through the courtyard gardens.

The Soft landscaping works shall include:

- i) Planting plans (at a scale not less than 1:100);
- ii) Written specification of planting and cultivation works to be undertaken;
- iii) Schedules of plants, noting species, plant sizes, plant container sizes (all at time of planting) and proposed plant numbers / densities and a landscape implementation programme; and
- iv) Details of tree planting / fixing details and fixing details for any climbing plants for the ground level and podium 1st floor courtyard level and any support required for the large shrubs on the 5th floor courtyard.

The hard landscape details shall include:

- i) Details, cross sections and elevations for the ground floor courtyard, 1st floor podium and fifth floor courtyards;
 - ii) Samples to show the texture and colour of the materials to be used, together with information about their sourcing/ manufacturer;
 - iii) Details and images, including information about their sourcing/ manufacturer of the ground floor hard landscape for the public realm; and
 - iv) Details, images, sections and locations of all furniture and seating elements, swing seat, raised beds including growing medium and drainage layers and built in seating, irrigation (if proposed), pergola, screens, decking areas and sun deck loungers.
-
- b) Cross sections through the details in the courtyards at a scale of not less than 1:50.
-
- c) Details, images, locations and general arrangement of all play elements and safety surfacing, including information about their sourcing/ manufacturer and a specification of all the play elements and surface treatment to be installed as set out in the Landscape Design development document, and arrangements for ensuring the safety and security of children using the play areas.
-

The development shall be implemented in accordance with the scheme so agreed and shall be retained as such thereafter.

REASON: To ensure that the development makes provision for hard and soft landscaping which contributes to the creation of a high quality, accessible, safe

and attractive public realm and to ensure a high standard of design, layout and amenity, in accordance with policies 7.4 and 7.21B of The London Plan (2016) and policies DM1 and DM22 of the Harrow Development Management Policies Local Plan (2013).

21. Brown Roof

The development other than demolition works, hereby permitted shall not commence until details of the brown roof shown on the approved plans shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a biodiversity based extensive substrate base which may be partly seeded with a selection of native plant species. The brown roof shall not be used as an amenity or sitting out space of any kind whatsoever and shall only be used in the case of essential maintenance or repair. The development shall be carried out strictly in accordance with the details so approved, shall be maintained as such thereafter and no change there from shall take place without the prior written consent of the Local Planning Authority.

REASON: To ensure the development provides the maximum possible provision towards creation of habitats and valuable areas for biodiversity and to ensure an acceptable impact on the character and appearance of the area in accordance with policies 7.4, 7.6, 5.10, and 7.19 of The London Plan (2016) and policies DM1, DM21 and DM22 of the Harrow Development Management Policies Local Plan (2013).

22. Inclusive Access Strategy

The development hereby approved shall not progress beyond first floor level until an inclusive access strategy for the site has been submitted to, and agreed in writing by, the Local Planning Authority. The strategy shall:

- (a) demonstrate inclusive access within the proposed development including the cinema and cafe unit;
- (b) detail the design of all gradients, ramps and steps within publicly accessible areas of the development; and
- (c) detail the arrangements for disabled residents' access to the disabled parking spaces and the access, and use of, waste and recycling facilities within the development.

The development shall be carried out in accordance with the approved details prior to the commencement of the use of the cinema or café or disabled parking spaces, whichever is the soonest and shall be retained as such thereafter.

Reason: To ensure that the development contributes to the achievement of a lifetime neighbourhood, in accordance with Policies 3.8 and 7.1 of the London Plan, Core Strategy Policy CS1 and Policy DM2 of the Development Management Policies Local Plan 2013

23. Biodiversity Management Plan

The development other than demolition works hereby permitted shall not commence until detailed proposals for ongoing management, monitoring and reporting of onsite biodiversity features, including details of how this will be funded in perpetuity have been submitted to and approved in writing by the Local Planning Authority. The Biodiversity Management Plan shall include:

- (a) how these features will be managed and monitored to maintain and enhance their value, including any replacements during the establishment phase (to run for three years from the commencement of construction);
- (b) year by year schedule of the timing of operations and who has responsibility for these and for ensuring they are undertaken;
- (c) how the plan will be updated at 5 year intervals; and
- (d) how these operations will be funded over the lifetime of the new development, during and following construction.

The development shall be carried out in accordance with the proposals so agreed and shall be retained as such thereafter.

Reason: To ensure that the development makes appropriate provision for the protection, enhancement, creation and management of biodiversity within the site and surrounding area in accordance with policy 7.19 of The London Plan (2016) and policy DM22 of the Harrow Development Management Policies Local Plan (2013).

24. Air Extraction System

The development other than demolition works hereby permitted shall not commence until full details and specifications of the proposed air extraction system, including details for the control of noise and odour emanating from the proposed air extraction system, has been submitted to and approved in writing by the Local Planning Authority. Thereafter, the air extraction system shall be implemented and maintained in full compliance with the approved details and shall be thereafter retained, unless otherwise agreed in writing by the Local Planning Authority.

REASON: In order to safeguard the amenities of neighbouring resident in accordance with policy 7.15 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

25. Landscape Management and Maintenance Plan

The development hereby approved shall not be first occupied until a Landscape Management and Maintenance Plan has been submitted to and approved in writing by the Local Planning Authority, for all landscaped areas and any landscape structure for plant growth, other than privately owned domestic balconies/terraces. The Landscape Management and Maintenance Plan shall include:

- a) Landscape Management: long term design objectives incorporating the overall functional aesthetic objectives of the landscape scheme; management responsibilities; programme of works and monitoring procedures; and
- b) Landscape Maintenance: including routine physical tasks required to satisfy appropriate standards of aftercare and enable the design and implementation objectives to be achieved, for the first year of maintenance, years 2-5 and 6 years onwards: maintenance responsibilities; a schedule of maintenance operations (calendar of tasks) set out graphically and in writing.

The Landscape Management and Maintenance Plan shall be carried out in accordance with the approved details.

REASON: To ensure the future success of the development and to enhance the appearance of the development, in accordance with policies 7.4 and 7.21B of The London Plan (2016) and policies DM1 and DM22 of the Harrow Development Management Policies Local Plan (2013).

26. Building Maintenance

The development hereby approved shall not be occupied until a strategy for maintaining the external surfaces of the building has first been submitted to and agreed in writing by the Local Planning Authority. The strategy shall include details of the regime for cleaning, re-rendering and repairing the buildings and the logistical arrangements for implementing that regime. Maintenance of the external surfaces of the buildings shall adhere to the strategy so agreed.

REASON: To ensure that maintenance of the development is carried out to preserve the highest standards of architecture and materials in accordance with Policies 7.6 and 7.7 of the London Plan (2016) and Policies AAP 4 and AAP 6 of the Harrow and Wealdstone Area Action Plan (2013).

27. Communal Aerials

Prior to the first occupation of the residential units hereby permitted, details of a strategy for the provision of communal facilities for television reception (eg. aerials, dishes and other such equipment) shall be submitted to and approved in writing by the Local Planning Authority. Such details shall include the specific size and location of all equipment. The approved details shall be implemented prior to the first occupation of the residential units and shall be retained thereafter. No other television reception equipment shall be installed on the building without the prior written approval of the Local Planning Authority.

REASON: To ensure that any telecommunications apparatus and other plant or equipment that is required on the exterior of the buildings preserves the high quality design of the buildings and spaces in accordance with policy 7.4 of the London Plan (2016), and DM49 of the Development Management Policies Local Plan (2013), and to ensure that the development achieves a high standard of amenity for future occupiers the buildings in accordance with policy DM 1 of the Development Management Policies Local Plan (2013).

28. Secured by Design

Evidence of certification of Secured by Design Accreditation for the development shall be submitted to and approved in writing by the Local Planning Authority before any part of the development is occupied or used.

REASON: In the interests of creating safer and more sustainable communities and to safeguard amenity by reducing the risk of crime and the fear of crime in accordance with policy 7.6 of the London Plan (2016) and policy DM1 of the Development Management Policies Local Plan (2013).

29. Parking Design and Management Plan

The flats hereby approved shall not be first occupied until a detailed parking design and management plan is submitted to and approved in writing by the Local Planning Authority to set out how parking will be allocated / controlled. The parking management plan shall set out details of disabled parking provision; active and passive electric vehicle charging points in accordance with London Plan standards. The arrangements so agreed shall be put in place prior to the first occupation of the flats hereby approved and shall thereafter be retained.

To ensure appropriate parking provision in accordance with the National Planning Policy Framework (2019), Policy 6.13 of the London Plan (2016) and Policy DM 42 of the Harrow Development Management Policies Local Plan (2013).

REASON: To ensure car parking provision is available for use by the occupants and visitors of the site and in accordance with policy T6 of The Draft London Plan (2019) and policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

30. Car Parking

The residential units hereby permitted shall not be occupied until three accessible parking spaces of a standard size have been clearly marked out on site, in accordance with the approved ground floor plan (IF-18-04/100 Rev:A) Such spaces shall not be used for any purposes other than for the parking of motor vehicles used by residents of the development for blue badge holders/disabled persons only, and for no other purpose, unless otherwise agreed in writing by the Local Planning Authority.

REASON: To ensure car parking provision is available for use by the occupants of the site and in accordance with policy T6 of The Draft London Plan (2019) and policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

31. Residential Cycle Parking

The residential units hereby permitted shall not be occupied until details for secured, sheltered and accessible storage for the provision of 145 long-stay cycle spaces and 2 short-stay cycle spaces (including non-standard bicycles) have been submitted and approved in writing by the Local Planning Authority. Such cycle storage should be designed in accordance with the requirements of the London Cycle Design Standards. The development shall be completed in accordance with the approved details prior to the occupation of the residential units, and shall thereafter be retained.

REASON: To ensure that cycle storage is available for use by the occupants of the site in accordance with policy T5 of The Draft London Plan (2019) and policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

32. Non-Residential Cycle Parking

The cinema and cafe hereby permitted shall not commence operations until details for secured, sheltered and accessible storage for the provision of 3 long-stay cycle spaces and 10 short-stay cycle spaces (including non-standard bicycles) have been submitted and approved in writing by the Local Planning Authority. Such cycle storage should be designed in accordance with the requirements of the London Cycle Design Standards. The development shall be completed in accordance with the approved details prior to the occupation of the residential units, and shall thereafter be retained.

REASON: To ensure that cycle storage is available for use by the occupants of the site in accordance with policy T5 of The Draft London Plan (2019) and

policies DM1 and DM42 of the Harrow Development Management Policies Local Plan (2013).

33. Bat Roosting/Hibernation Shelters

Prior to first occupation of the development hereby approved, details of the following shall be submitted to and approved in writing by the Local Planning Authority:

- (a) 9 Schwegler 2FTR interconnecting bat tubes (or equivalent), placed in three groups of three, at heights of 3 and 5 storeys above ground level on the south aspect of the new building, away or otherwise sheltered from direct night time illumination.

The bat roosts shall be installed in accordance with the approved details, and shall be retained as such thereafter.

REASON: To ensure that the development makes appropriate provision for the protection, enhancement, creation and management of biodiversity in accordance with policies 7.19 of The London Plan (2016) and policies DM1, DM21 and DM22 of the Harrow Development Management Policies Local Plan (2015).

34. Bird Boxes

Prior to first occupation of the development hereby approved, details of the following shall be submitted to and approved in writing by the Local Planning Authority:

- (a) 5 double or treble cavity swift nest boxes (e.g. Schwegler 17B)
- (b) 3 house sparrow terraces (e.g. Schwegler 1SP)
- (c) 3 nesting cavities for starlings (e.g. Schwegler Brick Box Type 24 with the entrance cover removed/enlarged)
- (d) 1 kestrel nestbox (e.g. Schwegler Built-in Multi-system cavity with kestrel front panel)

The bird boxes shall be embedded within the fabric of the building, in locations that take account of each species preferences, on north or east building aspects. The bird boxes shall be installed in accordance with the approved details, and shall be retained as such thereafter.

REASON: To ensure that the development makes appropriate provision for the protection, enhancement, creation and management of biodiversity in accordance with policies 7.19 of The London Plan (2016) and policies DM1, DM21 and DM22 of the Harrow Development Management Policies Local Plan (2015).

35. Invertebrate shelters

Prior to first occupation of the development hereby approved, details of the following shall be submitted to and approved in writing by the Local Planning Authority:

- 2+ cedar 'slab shelters to be constructed and fitted to the exterior on the south and east aspects, as follows: Sheets of (c. 2+ cm thick and c. W. 20+ x L. 30+ cm) untreated cedar board screwed to exterior wall on S aspect, with two 0.5-0.8 cm (by 2.5 x 2.5) 'lugs' at glued to the bottom on the wall side (to provide a slight offset from the wall which will narrow toward the top. These will provide shelter for a range of other invertebrates.

The invertebrate shelters shall be installed in accordance with the approved details, and shall be retained as such thereafter.

REASON: To ensure that the development makes appropriate provision for the protection, enhancement, creation and management of biodiversity in accordance with policies 7.19 of The London Plan (2016) and policies DM1, DM21 and DM22 of the Harrow Development Management Policies Local Plan (2015)

36. Refuse Stores – Residential

The residential refuse stores as shown on drawing number IF84-04/100 Rev:A shall be provided prior to the first occupation of the flats and shall be thereafter retained. The refuse stores shall have direct access from the public highway and shall have keypad code access installed on the external door (a keyless door), unless the prior written approval of the Local Planning Authority is first obtained.

REASON: To ensure satisfactory refuse stores are provided, which are accessible to refuse collectors, in accordance with policies DM1 and DM45 of the Harrow Development Management Policies Local Plan (2013).

37. Refuse Stores – Trade Waste

The trade waste refuse stores as shown on drawing number IF84-04/100 Rev:A shall be provided prior to the commencement of the use of the cinema or café, whichever is the soonest and shall be thereafter retained. The refuse store shall have keypad code access installed on the external door (a keyless door), unless the prior written approval of the Local Planning Authority is first obtained.

REASON: To ensure satisfactory refuse stores are provided, which are accessible to refuse collectors, in accordance with policies DM1 and DM45 of the Harrow Development Management Policies Local Plan (2013).

38. Refuse Bin Storage

The refuse bins shall be stored at all times, other than on collection days, in the designated refuse storage area, as shown on the approved drawing plans.

REASON: To safeguard the appearance and character of the surrounding area, in accordance with policy 7.4.B of The London Plan (2016) and ensure a high standard of residential quality in accordance with policies DM 1 and DM 45 of the Harrow Development Management Policies Local Plan (2013).

39. Part M

A minimum of 10% of the units shall be built in accordance with Building Regulation standard M4 (3) 'Wheelchair User Dwellings'. All other residential units in this development, as detailed in the submitted and approved drawings, shall be built to Building Regulation Standard M4 (2) 'Accessible and adaptable dwellings'. The development shall be thereafter retained to those standards.

REASON: To ensure provision of 'Wheelchair and Accessible and adaptable' housing in accordance with policies 3.8 and 7.2 of The London Plan (2016) and policy DM 2 of the Harrow Development Management Policies Local Plan (2013)

40. Permitted Development Restrictions - Cinema

The cinema (Use Class D2) and ancillary café hereby permitted, shall only be used for the purposes as set out in the application and for no other purposes, including any other purposes in Class D2 of Schedule 1 to the Town and Country Planning (Use Classes) Order 1987 (or in any provision equivalent to that Class in any Statutory Instrument revoking and re-enacting that order without modification).

REASON: To enable the Local Planning Authority to fully consider the effects of development normally permitted by the Town and Country Planning (General Permitted Development) Order 2015 to maintain mixed, balanced, sustainable and inclusive communities in accordance with policy 7.1 of the London Plan (2016) and policy DM1 of the Harrow Development Management Polices Local Plan (2013).

41. Permitted Development Restrictions – Residential

The flats hereby permitted shall be used for Class C3 dwellinghouses(s) only and notwithstanding the provisions of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking or re-enacting that Order with or without modification), no development within Schedule 2, Part 3, Class L shall take place.

REASON: To enable the Local Planning Authority to fully consider the effects of

development normally permitted by the Town and Country Planning (General Permitted Development) Order 2015 to maintain mixed, balanced, sustainable and inclusive communities in accordance with policy 7.1 of the London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

42. Permitted Development Restrictions – Communications

Notwithstanding the provisions of Part 16 (Communications) to Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015, or any order revoking and replacing that Order with or without modification, no development that would otherwise be permitted by that part of the Order (or the equivalent provisions of any replacement Order) shall be carried out without planning permission having first been obtained by the Local Planning Authority.

REASON: To ensure that the architectural integrity of the building is not compromised, in accordance with policy 7.6 of the London Plan (2016) and policy DM 1 of the Harrow Development Management Policies Local Plan (2013).

43. Audio Visual Control

The flats hereby approved shall not be occupied until: (i) an audio-visual access control system has been installed; or (ii) such alternative security measures have been installed that shall first have been submitted to, and agreed in writing by, the Local Planning Authority.

REASON: To ensure that the development achieves a high standard of residential quality for future occupiers of the development in accordance with Policy 3.5 of the London Plan (2015) and policy DM1 of the Development Management Policies Local Plan (2013).

44. Noise and Vibration – Certification

Within three months following the completion of the installation of the noise and vibration mitigation measures (as per the approved details subject to condition 10 of this permission), the applicant shall submit written evidence to the Local Planning Authority to confirm that the works have been carried out and completed in accordance with the approved details, as per condition 10 of this permission.

REASON: In order to safeguard the amenities of neighbouring residents in accordance with policy 7.15 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

45. Hours of Operation

The opening hours of the cinema (screens 1-3) and the café shall be between:

- 09:00 hrs to 23:00 Sundays to Thursdays (inclusive) and public holidays and;
- 09:00hrs to 24:00 hrs on Fridays and Saturdays.

The opening hours of the Cinema – Screen 4/Workshop, shall be between:

- 09:00 hrs to 22:00 Sundays to Thursdays (inclusive) and public holidays and;
- 09:00hrs to 23:00 hrs on Fridays and Saturdays.

REASON: In order to safeguard the amenities of neighbouring resident in accordance with policy 7.15 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

46. Background Noise Level

The rating level of the noise emitted from the site shall be at least 10dB below the existing background noise level. The noise levels shall be determined at the boundary of the nearest noise sensitive premises. The measurements and assessment shall be made in accordance to the latest British Standard 4142, 'Method for rating industrial noise affecting mixed residential and industrial areas'.

REASON: In order to safeguard the amenities of neighbouring resident in accordance with policy 7.15 of The London Plan (2016) and policy DM1 of the Harrow Development Management Policies Local Plan (2013).

47. Deliveries

Deliveries for the cinema and café unit shall take place between the following times only:- 07:00 – 17:00 Monday to Saturday and at no time on Sundays and Bank Holidays.

REASON: To safeguard the amenities of neighbouring properties within the locality, in accordance with policy DM1 of the Harrow Development Management Local Policies Plan.

48. Landscape Implementation

All planting, seeding or turfing comprised in the approved details of landscaping shall be carried out in the first planting and seeding seasons following the occupation of the building(s), or the completion of the development, whichever is the sooner. Any existing or new trees or shrubs which, within a period of 5 years from the completion of the development, die, are removed, or become seriously damaged or diseased, shall be replaced in the next planting season, with others of a similar size and species, unless the Local Planning Authority agrees any variation in writing.

REASON: To safeguard the appearance and character of the area, and to enhance the appearance of the development, in accordance with policies 7.4 and 7.21B of The London Plan (2016) and policies DM1, DM22 and DM23 of the Harrow Development Management Policies Local Plan (2013).

49. Post-Construction Assessment

The development shall be undertaken in accordance with the approved Sustainability Statement and Energy Strategy. Within 3 months (or other such period agreed in writing by the Local Planning Authority) of the final completion of the development a post construction assessment shall be undertaken demonstrating compliance with the approved Energy Statement by JS Lewis Ltd (dated July 2019), which thereafter shall be submitted to the Local Planning Authority in writing to be agreed the Local Planning Authority for written approval.

REASON: To ensure the delivery of a sustainable development in accordance with the National Planning Policy Framework (2019), policies 5.2.B/C/D/E of The London Plan (2016) and policies DM12, DM13 and DM14 of the Harrow Development Management Policies Local Plan (2013).

50. Landscaping – Completion

Within three months following the completion of the soft and hard landscaping works as per the approved details, Macfarlane & Assocs shall submit written and photographic evidence to the Local Planning Authority to confirm that the works have been carried out and completed in accordance with the approved details, as per conditions 41 and 42 of this permission.

REASON: To safeguard the appearance and character of the area, and to enhance the appearance of the development, in accordance with policies 7.4 and 7.21B of The London Plan (2016) and policies DM1, DM22 and DM23 of the Harrow Development Management Policies Local Plan (2013).

INFORMATIVES

1. Relevant Policies

The following policies are relevant to this decision:

National Planning Policy Framework (2019)

The London Plan (2016):

2.15, 3.3, 3.4, 3.5C, 3.6, 6.7, 3.8, 3.9, 3.11, 3.12, 3.13, 4.12, 5.2, 5.3, 5.10, 5.11, 5.13, 5.14, 5.15, 5.21, 6.3, 6.9, 6.13, 7.1, 7.2, 7.3B, 7.4B, 7.5, 7.6B, 7.7, 7.8, 7.13, 7.14, 7.15, 7.19, 7.21

The Draft London Plan (2019):

D1, D2, D3, D4, D5, D6, D7, D8, D9, D11, D12, D13, H1, H2, H5, H8, H12, HC1, G5, G6, G7, SI1, SI2, SI3, SI5, SI8, SI13, T2, T3, T4, T5, T6, SD6, SD7

Harrow Core Strategy (2012):

CS1

Harrow and Wealdstone Area Action Plan (2013)

AAP2, AAP8, AAP13

Harrow Development Management Policies Local Plan (2013):

DM1, DM2, DM3, DM7, DM9, DM10, DM12, DM13, DM14, DM15, DM20, DM21, DM22, DM24, DM27, DM28, DM35, DM40, DM41, DM42, DM43, DM44, DM45, DM46, DM47, DM49, DM50

Harrow and Wealdstone Area Action Plan (2013)

AAP1, AAP2, AAP4, AAP6, AAP13

Supplementary Planning Documents:

Supplementary Planning Document: Residential Design Guide (2010)

Supplementary Planning Document: Planning Obligations and Affordable Housing (2013)

The London Plan Housing Supplementary Planning Guidance (2016)

Supplementary Planning Document : Accessible Homes (2010)

Technical Housing Standard (2015) Code of Practice for Storage and Collection of Refuse and Materials for Recycling in Domestic Properties (2008).

Mayors Housing Supplementary Planning Guidance (2016)

Mayors Affordable Housing and Viability Supplementary Planning Guidance (2017)

2. Pre-application engagement

Statement under Article 35(2) of The Town and Country Planning (Development Management Procedures) (England) Order 2015

This decision has been taken in accordance with paragraphs 187-189 of The National Planning Policy Framework. Pre-application advice was sought and provided and the submitted application was in accordance with that advice.

3. Mayoral CIL

Please be advised that approval of this application (either by Harrow Council, or subsequently by the Planning Inspectorate if allowed on appeal following a refusal by Harrow Council) will attract a Community Infrastructure Levy (CIL) liability, which

is payable upon the commencement of development. This charge is levied under s.206 of the Planning Act 2008 Harrow Council, as CIL collecting authority, has responsibility for the collection of the Mayoral CIL The Provisional Mayoral CIL liability for the application, based on the Mayoral CIL levy rate for Harrow of £60/sqm is £250,884.00

The floorspace subject to CIL may also change as a result of more detailed measuring and taking into account any in-use floor space and relief grants (i.e. for example, social housing).

You are advised to visit the planningportal website where you can download the appropriate document templates.

Please complete and return the Assumption of Liability Form 1 and CIL Additional Information Form 0 .

https://ecab.planningportal.co.uk/uploads/1app/forms/form_1_assumption_of_liability.pdf

https://ecab.planningportal.co.uk/uploads/1app/forms/cil_questions.pdf

If you have a Commencement Date please also complete CIL Form 6:

https://ecab.planningportal.co.uk/uploads/1app/forms/form_6_commencement_notice.pdf

The above forms should be emailed to HarrowCIL@Harrow.gov.uk

Please note that the above forms must be completed and provided to the Council prior to the commencement of the development; failure to do this may result in surcharges and penalties

4. Harrow CIL

Harrow has a Community Infrastructure Levy which applies Borough wide for certain developments of over 100sqm gross internal floor space.

Harrow's Charges are:

Residential (Use Class C3) - £110 per sqm;

Hotels (Use Class C1), Residential Institutions except Hospitals, (Use Class C2), Student Accommodation, Hostels and HMOs (Sui generis) - £55 per sqm;

Retail (Use Class A1), Financial & Professional Services (Use Class A2), Restaurants and Cafes (Use Class A3) Drinking Establishments (Use Class A4) Hot Food Takeaways (Use Class A5) - £100 per sqm

All other uses - Nil.

The Provisional Harrow CIL liability for the application, based on the Harrow CIL levy rate for Harrow of £110/sqm is £564,623.55

This amount includes indexation which is 323/224. The floorspace subject to CIL may also change as a result of more detailed measuring and taking into account any in-use floor space and relief grants (i.e. for example, social housing).

The CIL Liability is payable upon the commencement of development.

You are advised to visit the planningportal website where you can download the relevant CIL Forms.

Please complete and return the Assumption of Liability Form 1 and CIL Additional Information Form 0 .

https://ecab.planningportal.co.uk/uploads/1app/forms/form_1_assumption_of_liability.pdf

https://ecab.planningportal.co.uk/uploads/1app/forms/cil_questions.pdf

If you have a Commencement Date please also complete CIL Form 6:
https://ecab.planningportal.co.uk/uploads/1app/forms/form_6_commencement_notice.pdf

The above forms should be emailed to HarrowCIL@Harrow.gov.uk

Please note that the above forms must be completed and provided to the Council prior to the commencement of the development; failure to do this may result in surcharges.

5. Considerate Contractor Code of Practice

The applicant's attention is drawn to the requirements in the attached Considerate Contractor Code of Practice, in the interests of minimising any adverse effects arising from building operations, and in particular the limitations on hours of working.

6. Party Wall Act

The Party Wall etc. Act 1996 requires a building owner to notify and obtain formal agreement from adjoining owner(s) where the building owner intends to carry out building work which involves:

1. work on an existing wall shared with another property;
 2. building on the boundary with a neighbouring property;
 3. excavating near a neighbouring building,
- and that work falls within the scope of the Act.

Procedures under this Act are quite separate from the need for planning permission or building regulations approval.

"The Party Wall etc. Act 1996: Explanatory booklet" is available free of charge from: Communities and Local Government Publications, PO Box 236, Wetherby, LS23 7NB

Please quote Product code: 02 BR 00862 when ordering

Also available for download from the CLG website:

<http://www.communities.gov.uk/documents/planningandbuilding/pdf/133214.pdf>

Tel: 0870 1226 236 Fax: 0870 1226 237

Textphone: 0870 1207 405

E-mail: communities@twoten.com

7. Compliance with Planning Conditions

IMPORTANT: Compliance With Planning Conditions Requiring Submission and Approval of Details Before Development Commences - You will be in breach of planning permission if you start development without complying with a condition requiring you to do something before you start. For example, that a scheme or details of the development must first be approved by the Local Planning Authority. Carrying out works in breach of such a condition will not satisfy the requirement to commence the development within the time permitted.- Beginning development in breach of a planning condition will invalidate your planning permission.

- If you require confirmation as to whether the works you have carried out are acceptable, then you should apply to the Local Planning Authority for a certificate of lawfulness.

8. Liability For Damage to Highway

The applicant is advised to ensure that the highway is not interfered with or obstructed at any time during the execution of any works on land adjacent to a highway. The applicant is liable for any damage caused to any footway, footpath, grass verge, vehicle crossing, carriageway or highway asset. Please report any damage to nrswa@harrow.gov.uk or telephone 020 8424 1884 where assistance with the repair of the damage is available, at the applicants expense. Failure to report any damage could result in a charge being levied against the property.

9. Street Numbering

Harrow Council is responsible for the naming and numbering of new or existing streets and buildings within the borough boundaries. The council carries out these functions under the London Government Act 1963 and the London Building Acts (Amendment) Act 1939.

All new developments, sub division of existing properties or changes to street names or numbers will require an application for official Street Naming and Numbering (SNN). If you do not have your development officially named/numbered, then then it will not be officially registered and new owners etc. will have difficulty registering with utility companies etc.

You can apply for SNN by contacting technicalservices@harrow.gov.uk or on the following link.

[http://www.harrow.gov.uk/info/100011/transport and streets/1579/street naming and numbering](http://www.harrow.gov.uk/info/100011/transport%20and%20streets/1579/street-naming-and-numbering)

10. Thames Water Assets

The proposed development is located within 15m of Thames Water underground assets and as such the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary structures <https://developers/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>. Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB

11. Ground Water

A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. It is expected for the developer to demonstrate what measures will be undertaken to minimise ground water discharges not the public sewer.

Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 02035779483 or by emailing wwriskmanagement@thameswater.co.uk.

12. Surface Water Drainage

Thames Water would advise that if the developer follows a sequential approach to the disposal of water. Prior approval will be required for the discharge to a public sewer. For further information please visit Thames Water website.

13. Sustainable Drainage Systems

The applicant is advised that surface water run-off should be controlled as near to its source as possible through a sustainable drainage approach to surface water management (SUDS). SUDS are an approach to managing surface water run-off which seeks to mimic natural drainage systems and retain water on or near the site as opposed to traditional drainage approaches which involve piping water off site as quickly as possible.

SUDS involve a range of techniques including soakaways, infiltration trenches, permeable pavements, grassed swales, ponds and wetlands. SUDS offer significant advantages over conventional piped drainage systems in reducing flood risk by attenuating the rate and quantity of surface water run-off from a site, promoting groundwater recharge, and improving water quality and amenity.

Where the intention is to use soak ways they should be shown to work through an appropriate assessment carried out under Building Research Establishment (BRE) Digest 365.

Support for the SUDS approach to managing surface water run-off is set out in the National Planning Policy Framework (NPPF) and its accompanying technical guidance, as well as the London Plan. Specifically, the NPPF (2012) gives priority to the use of sustainable drainage systems in the management of residual flood risk and the technical guidance confirms that the use of such systems is a policy aim in all flood zones. Policy 5.13 of the London Plan (2016) requires development to utilise sustainable drainage systems unless there are practical reasons for not doing so. Sustainable drainage systems cover the whole range of sustainable approaches to surface drainage management. They are designed to control surface water run-off close to where it falls and mimic natural drainage as closely as possible. Therefore, almost any development should be able to include a sustainable drainage scheme based on these principles.

The applicant can contact Harrow Drainage Section for further information.

14. Designing Out Crime

For further information regarding Secure By Design, the applicant can contact the North West London Designing Out Crime Group on the following: DOCOMailbox.NW@met.police.uk

15. Network Management

The developer is urged to make early contact with Network Management in order to agree any temporary traffic management measures required; these should then be included in the detailed CLP for submission.

Interim Chief Planning Officer	
Corporate Director	

APPENDIX 2: GREATER LONDON AUTHORITY STAGE 1 RESPONSE

GREATER LONDON AUTHORITY

Beverley Kuchar
Interim Chief Planning Officer
Planning Services
Harrow Council
Civic Centre, Station Road
Harrow
HA1 2UY

Department: Planning
Your reference: P/3043/19
Our reference: GLA/5037/01/AJR
Date: 18 November 2019

Dear Beverley

Town & Country Planning Act 1990 (as amended); Greater London Authority Acts 1999 and 2007; Town & Country Planning (Mayor of London) Order 2008.

Safari Cinema, 2 Station Road, Harrow

Local planning authority reference: P/3043/19

I refer to the copy of the above planning application, which was received 16 September 2019. On 18 November 2019, the Deputy Mayor, acting under delegated authority, considered a report on this proposal, reference GLA/5037/01. A copy of the report is attached, in full. This letter comprises the statement that the Mayor is required to provide under Article 4(2) of the Order.

The Deputy Mayor considers that the application does not comply with the London Plan and draft London Plan, for the reasons set out in paragraph 72 of the above report. However, the resolution of those issues could lead to the application becoming compliant with the London Plan and draft London Plan.

If your Council subsequently resolves to make a draft decision on the application, it must consult the Mayor again under Article 5 of the Order and allow him fourteen days to decide whether to allow the draft decision to proceed unchanged, or direct the Council under Article 6 to refuse the application, or issue a direction under Article 7 that he is to act as the local planning authority for the purpose of determining the application and any connected application. You should therefore send me a copy of any representations made in respect of the application, and a copy of any officer's report, together with a statement of the decision your authority proposes to make, and (if it proposed to grant permission) a statement of any conditions the authority proposes to impose and a draft of any planning obligation it proposes to enter into and details of any proposed planning contribution.

City Hall, London, SE1 2AA • london.gov.uk • 020 7983 4000

Please note that the Transport for London case officer for this application is Juan Sanclemente, e-mail v_JuanSanclemente@tfl.gov.uk.

Yours sincerely



John Finlayson
Head of Development Management.

cc Navin Shah, London Assembly Constituency Member
Andrew Boff, Chair of London Assembly Planning Committee
National Planning Casework Unit, DCLG
Lucinda Turner, TfL

Safari Cinema

in the London Borough of Harrow

planning application no. P/3043/19

<p>Strategic planning application stage 1 referral</p> <p>Town & Country Planning Act 1990 (as amended); Greater London Authority Acts 1999 and 2007; Town & Country Planning (Mayor of London) Order 2008.</p>
<p>The proposal</p> <p>Redevelopment to provide part 5/part 11 storey building with basement level. Comprising 78 residential units, Cinema, ancillary cafe, parking, landscaping, bin and cycle stores.</p>
<p>The applicant</p> <p>The applicants are Asprea 2 Limited and the architect is IF Architects.</p>
<p>Strategic issues summary</p> <p>Principle: The principle of development could be supported subject to further information the re-provision and/or relocation of the existing church and the level of affordable housing within the scheme. (Paragraphs 14-28).</p> <p>Housing: The affordable housing offer is 13%, all intermediate tenure, which is considered to be wholly unacceptable and should be significantly increased. There is no clear justification for the single tenure offer and all opportunities for the scheme to deliver affordable rent must be explored. GLA officers are currently robustly scrutinising the viability appraisal to maximise the provision of affordable housing. Early and late stage review mechanisms must be secured. (Paragraphs 29-40).</p> <p>Urban design: The layout, height, massing and appearance of the scheme are broadly supported in accordance with 7.5 of the London Plan and Policies D1 and D2 of the draft London Plan. Layout changes regarding courtyard and wheelchair access are required, as is further consideration of the relationship between the application site and adjacent Tesco site. (Paragraphs 41-48).</p> <p>Further information, conditions and s106 obligations are required in respect of energy, drainage, urban greening and transport. (Paragraphs 49-60).</p>
<p>Recommendation</p> <p>That Harrow Council be advised that the scheme does not currently comply with the London Plan for the reasons set out in paragraph 72 of this report, but that the possible remedies set out in that paragraph could address these deficiencies.</p>

Context

1 On 16 September 2019, the Mayor of London received documents from Harrow Council notifying him of a planning application of potential strategic importance to develop the above site for the above uses. Under the provisions of The Town & Country Planning (Mayor of London) Order 2008, the Mayor will provide the Council with a statement setting out whether he considers that the application complies with the London Plan, and his reasons for taking that view. The Mayor may also provide other comments. This report sets out information for the Mayor's use in deciding what decision to make.

2 The application is referable under Category 1C of the Schedule to the 2008 Order:

- 1C(c): "Development which comprises or includes the erection of a building, where the building is more than 30 metres high and is outside the City of London."

3 Once Harrow Council has resolved to determine the application, it is required to refer it back to the Mayor for his decision as to whether to direct refusal; take it over for his own determination; or allow the Council to determine it itself.

4 The Mayor of London's statement on this case will be made available on the GLA website www.london.gov.uk.

Site description

5 The site constitutes an area of approximately 0.19 hectares of land situated on the western side of Station Road, at the junction with High Mead which bounds the site to the north. The site is presently occupied by a former cinema building, Safari Cinema, the construction of which dates from the mid 1930's. The existing cinema building is not subject of a heritage listing and the application site is not situated within a designated conservation area. Notwithstanding this, the original art deco frontage of the cinema (presently obscured behind metal cladding) is recognised by Harrow Council as having local significance.

6 On its southern boundary the site adjoins a series of three-storey terraced properties with frontage to Station Road, exhibiting commercial shops and offices at ground floor with residential flats situated above. To the west the application site adjoins a large Tesco Megastore situated on its northern portion and open car-parking areas across the remainder of the site.

7 Harrow and Wealdstone station is located approximately 600 metres to the north of the site, which is served by both underground (Bakerloo Line) and Overground services. Seven bus routes operate in the surrounding area and the site has a Public Transport Access Level (PTAL) of 5 on a scale of 0-6b, where 6b is excellent.

Details of the proposal

8 The proposal involves the demolition of the rear of the existing cinema building, and redevelopment of the site to provide a building ranging 5-11 storeys in height, which contains:

- 78 residential apartments on upper floors (C3 use);
- A 1,297 sq.m arthouse cinema (D2 use) at ground floor;
- 129 sq.m of café floorspace (ancillary to cinema) at first and second floors;
- Upgrade and retention of the existing art deco cinema façade;
- Basement car parking and servicing areas.

Case history

9 In 2015, planning approval was granted by Harrow Council (LPA Reference: P/4933/14) for a change of use submitted on behalf of the V2V Church, from Bingo Hall (Class D2) to combined community uses (including Community Café, Dance Classes, Food Bank, Life Skills, Training, Mothers and Toddlers Group) and Place of Worship (D1 Use) and Assembly/Leisure Use (Class D2).

10 The scheme has been subject to pre-application advice. An advice note (GLA 5037) was issued in May 2019 which outlined GLA officer's broad support for the proposal, subject to clarification of the quantum employment uses of the site, re-provision or relocation of the existing church use on the site, a suitable quantum and tenure of affordable housing being proposed; and further details provided in respect of design treatment, sustainability and transport considerations.

Strategic planning issues and relevant policies and guidance

11 For the purposes of Section 30(6) of the Planning and Compulsory Purchase Act 2004, the development plan in force for the area comprises the Harrow Core Strategy (2012), the Harrow Development Management Policies (2013), the Harrow Site Allocations (2013) and the 2016 London Plan (Consolidated with Alterations since 2011).

12 The following are relevant material considerations:

- The National Planning Policy Framework;
- National Planning Practice Guidance;
- Draft London Plan – consolidated suggested changes version (July 2019), which should be taken into account on the basis explained in the NPPF;
- The Mayor's Affordable Housing and Viability SPG which now must be read subject to the decision in *R (McCarthy and Stone) v the Mayor of London*

13 The relevant issues and corresponding policies are as follows:

- | | |
|----------------------------|--|
| • Principle of development | <i>London Plan;</i> |
| • Housing | <i>London Plan; Affordable Housing and Viability SPG; Housing SPG; Housing Strategy</i> |
| • Urban Design | <i>London Plan; Shaping Neighbourhoods: Character and Context SPG; Accessible London: Achieving an Inclusive Environment SPG; Shaping Neighbourhoods: Play and Informal Recreation SPG</i> |
| • Sustainable Development | <i>London Plan; Sustainable Design & Construction SPG; Mayor's Environment Strategy;</i> |
| • Transport | <i>London Plan; Mayor's Transport Strategy.</i> |

Principle of development

Church and community use

14 London Plan Policy 3.16 and Policy S1 of the draft London Plan seek to protect and enhance social infrastructure provision, including faith facilities, and resists proposals that would result in the loss of social infrastructure in areas of defined need for that type of facility without realistic proposals for re-provision. Policy SD6 of the draft London Plan states this to be particularly important in town centres, where the provision of social infrastructure should be enhanced, and facilities should be located in places that maximise footfall to surrounding town centre uses.

15 An area of 1,562 sq.m within the existing Safari Cinema complex is presently occupied by the V2V Church. The 2015 planning approval (referenced above) authorises the church to operate a mixture of community uses including a Community Café, Dance Classes, a Food Bank, Life Skills Training, a Mothers and Toddlers Group, Place of Worship and Assembly/Leisure Use therefore this is considered to be the lawful use of the site.

16 The applicant was advised at pre-application stage that re-provision of the existing church use within the proposal would be required, or an effective relocation strategy for the existing church uses elsewhere in the surrounding area must be provided, in order to satisfy the requirements of Policy 3.16 of the London Plan and S1 of the draft London Plan. Notwithstanding this, the current proposal does not provide either the re-provision of existing church facilities or any comprehensive strategy for their relocation.

17 The applicant has highlighted that the existing church congregation is drawn from a broad geographical area, and on this basis has identified five potential alternative sites which may be suitable for the existing church to be relocated to between 5 and 20 kilometres away from the existing site. Whilst the identification of alternative sites is welcomed, no detailed analysis has been provided on the feasibility or suitability of relocating the existing church functions to any of the identified sites.

18 In the above policy context, and in the absence of a commitment to re-provide the existing church community uses on the application site, a detailed relocation strategy must be provided for the existing church use prior to enable the GLA to confirm the acceptability of the sites redevelopment. Both the strategy and its implementation must be secured through conditions of approval and s106 obligations attached to any approval.

19 Separate further consideration of the implications of the proposed removal of the church under the Equalities Act is provided below.

Commercial uses

20 The application site is situated within the Harrow Metropolitan Town Centre, where Policies 2.15 of the London Plan and SD6 and SD7 of the draft London Plan seek to support housing growth in unison with enhancements to the competitiveness, quality and diversity of town centre retail, employment, arts, cultural consumer and public services.

21 The existing Safari Cinema building is primarily utilized as a Bollywood cinema, however is also used as a place of worship and community centre for the V2V Church, which has planning permission for the shared use of the site. The proposed mixed-use scheme incorporates the provision of 1,426 sq.m of commercial uses, including a 1,297 sq.m art-house cinema and a 129 sq.m ancillary café.

22 The proposal includes the re-provision of a cinema within the scheme which is strongly supported. Whilst the overall quantum of commercial cinema floorspace is reduced from 3,100 sq.m (as existing) to 1,426 sq.m (as proposed), the applicant has suitably demonstrated that the proposed art-house cinema will comprise a comparable commercial offering, improving on the existing cinema layout which comprises a significant amount of redundant and inefficient floorspace. The applicant has similarly demonstrated, through the provision of an economic statement, that the proposal would result in a net improvement to employment generation on the site with the creation of 9 additional jobs, which is in accordance with Policies 2.15 of the London Plan and SD6 and SD7 of the draft London Plan.

Housing

23 Policy 3.3 of the London Plan recognises the pressing need for more homes in London and sets a 10 year target for Harrow of 5,927 additional homes between 2015 and 2025. This increases to a 10 year target of 13,920 homes per year in Policy H1 of the draft London Plan, recognising the increasing demand for the delivery of new homes throughout London.

24 The application site is situated within the Harrow and Wealdstone Opportunity Area, which London Plan Policy 2.3 identifies as having the capacity to accommodate 3,000 new jobs and 2,000 new homes. These targets are revised to a capacity to deliver 1,000 new jobs and 5,000 new homes within Policy SD1 of the draft London Plan.

25 The proposal would result in the delivery of 70 new homes on the application site, which is welcomed, however in accordance with London Plan Policy 3.4 and draft London Plan Policy D1(B) developments must make the most efficient uses of land and be development at the optimum density, having regard to the design-led approach which determines the capacity of sites in line with core objectives of the London Plan and draft London Plan. In this instance, given the low level and mono-tenure of the affordable housing offer, GLA officers have concerns that the proposal fails to appropriately optimise the site in order to deliver a policy compliant level of affordable housing. The applicant and Council must work with GLA officers as a matter of priority to address this issue before the principle of residential on the site can be supported.

Equality Act 2010

26 The Equality Act 2010 requires public authorities to have due regard to the need to a) eliminate discrimination, harassment, victimisation and any other conduct that is prohibited under the Act; b) advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it; c) foster good relations between persons who share a relevant protected characteristic and persons who do not share it. The protected characteristics set out in the Equality Act 2010 are: age, disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

27 The applicant has submitted an Equalities Impact Assessment (EQIA) with the application. The assessment identifies 10 impacts (positive, negative and neutral) of the proposed development on people with protected characteristics. The EQIA notes that it is understood that the existing church congregation consists disproportionately of people from BAME backgrounds. Subsequently, the report concludes that the loss of the church would represent a long-term negative impact on Harrow BAME residents.

28 Relocation of the church to an alternative site (not within walking distance of the current site) is identified as a potential mitigation of the adverse impact identified above. However, this raises a further question about the potential impact of relocation on those currently using the church who may have mobility issues, due to age or disability. In the circumstances, without a detailed relocation strategy (secured by s.106 agreement obligations and the imposition of suitable planning conditions as necessary) and due consideration of the impacts of relocation on groups with protected characteristics, there is not sufficient information available to justify a categorisation of the impact as "minor". Further consideration of these matters is required to comply with the requirements of the Equality Act 2010.

Housing

Affordable housing

29 London Plan Policies 3.11 and 3.12, draft London Plan Policies H5 and H6, and the Mayor's Affordable Housing and Viability SPG set a strategic target of 50% affordable housing in all new developments. The Mayor's Affordable Housing and Viability SPG and Policy H6 of the draft London Plan set out the 'threshold approach' to planning applications whereby schemes that meet specific threshold of affordable housing by habitable room (in this case 35%) without public subsidy, and which meet the Mayor's preferred tenure mix, are eligible to follow the Fast-Track Route whereby viability information is not required and a late stage review would not be sought. The preferred tenure mix is outlined in Policy H7 of the draft London Plan as at least 30% low cost rent (social rent or affordable rent), at least 30% intermediate (London Living Rent or shared ownership) and the remaining 40% as determined by the local planning authority.

30 The proposal includes an affordable housing offer of 13% by habitable room (9 units), all of which are proposed as intermediate (shared ownership) tenure. This offer on a town centre site within an Opportunity Area is considered to be wholly unacceptable and must be significantly increased. The applicant has not provided any justification for the mono-tenure and every effort must be made to accommodate affordable rent within the proposal.

31 As the offer falls significantly short of the 35% Fast Track threshold, as outlined in policies H5 and H6 of the draft London Plan, and it must follow a viability tested route. The applicant has submitted a financial viability appraisal (FVA) and an initial review by GLA officers indicates that further provision of affordable housing can be reasonably achieved in the development. Detailed comments on the submitted FVA have been forwarded to the applicant under separate cover in this regard, and GLA officers will continue to robustly scrutinise the viability appraisal to ensure that the maximum amount of affordable housing is delivered. Both early and late stage review mechanisms must also be secured in accordance with the draft London Plan.

32 The affordability of the units must accord with the requirements of Policy H7 of the draft London Plan, the Mayor's Affordable Housing and Viability SPG and the London Plan Annual Monitoring Report. GLA officers will work with the Council to agree affordability levels of all affordable tenures as part of the review of viability. A draft of the Section 106 agreement must be provided to the GLA for review as soon as one is made available to ensure that the requisite review mechanisms have been applied (as required by draft Policy H6) and affordability and eligibility criteria comply with the London Plan, draft London Plan and the Mayor's Affordable Homes Programme Funding Guidance.

33 The applicant must also investigate the additional uplift of affordable housing provision through grant funding. Information confirming that this has been investigated must be submitted prior to the Mayor's consideration of the application at Stage 2.

Residential unit size mix

34 London Plan policies 3.6 and 3.11, as well as draft London Plan policy H12, encourage a choice of housing based on local needs with affordable family housing stated as a strategic priority. The scheme proposes a total of 78 units of which 3 (4%) are studio units, 17 (22%) are 1 bed units, 43 (55%) are one bed units and 15 (19%) are 3 bed units.

35 The proportion of family sized units is acceptable in this instance, noting the town centre location of the application site. Notwithstanding this, the applicant must demonstrate that family

sized accommodation is concentrated within the affordable housing elements of the scheme. To this end a full accommodation schedule must be provided which details the tenure of residential units.

Residential quality

36 London Plan Policy 3.5 and Policies D2 and D4 of the draft London Plan seek to ensure housing developments are of the highest quality internally, externally, and in relation to their context and to the wider environment. The Housing SPG and London Housing Design Guide build on this approach and provide further detailed guidance on key residential design standards. The residential units are generally suitably sized, and the scheme layout ensures an acceptable number of units per core is achieved. One 1 Bed flat on the 3rd floor which has a 'juliet' balcony, the applicant should aim to oversize this unit to compensate for having no external amenity space.

Children's playspace

37 London Plan Policy 3.6 and draft London Plan Policy S4 set out the expectation that housing proposals should make provision for play and informal recreation, based on the expected child population generated by the scheme and an assessment of future needs.

38 Using the methodology within the Mayor's Shaping Neighbourhoods: Play and Informal Recreation SPG, it is anticipated that there will be between 17 and 33 children within the development based on current housing mix. The guidance sets a benchmark of 10 sq.m. of useable child playspace to be provided per child, with under-5 year olds playspace provided on-site as a minimum. As such the development should make provision for between 170 sq.m and 330 sq.m. of playspace within the application site boundaries (dependent on unit size/tenure breakdown).

39 The applicant has identified a total of 167 sq.m of playspace provided across the first and fifth floor amenity spaces which falls below the minimum requirements outlined above. The applicant must provide a full accommodation schedule, including tenure breakdown, in order to determine the final anticipated child yield of the development. The applicant must quantify that the minimum required playspace will be provided on site, or alternatively a suitable contribution made to off-site play spaces within the surrounding locality. Where upgrades to off-site play areas are deemed appropriate, suitable contribution towards these must be secured via planning conditions and the s106 agreement.

40 The quality of design and equipment provided in these areas must be secured by planning conditions. The applicant must demonstrate that all playspace is available to all children within the development and not segregated by tenure type, in accordance with Policy S4 of the draft London Plan. The ongoing availability of all playspace to all children in the development must be secured by appropriate conditions of approval.

Urban Design

41 The design principles of chapter seven in the London Plan and chapter three of the draft London Plan outline that all developments should achieve a high standard of design which responds to local character, enhances the public realm and provides architecture of the highest quality. In particular, Policy 7.5 of the London Plan and Policies D1 & D2 of the draft London Plan require architecture to make a positive contribution to a coherent public realm, streetscape and wider cityscape, incorporating the highest quality materials and design appropriate to the surrounding context.

42 The proposed scheme was previously subject to pre-application advice, and this has helped to inform the design approach to the proposal adopted by the applicant which is welcomed. The height, massing and materiality of the scheme is broadly supported, as is the retention and improved

treatment of the existing art deco cinema façade.

Site layout

43 The 5th floor roof terrace is only accessible from the north eastern building core. Ensuring the other buildings have access to this play area would contribute to tenure blind play areas as required by Policy 54 of the draft London Plan. The remaining 2 building cores have access to the 1st floor podium courtyard, where the café also has access during opening hours, potentially compromising privacy and equality of space. At pre-application stage it was advised to maintain separation (including access) between the public café and residential courtyard and the scheme should be amended to reflect this.

44 Wheel chair accessible units currently have a convoluted route from the car parking bays, and this should be improved with a more direct route through the building and courtyard.

Rear block

45 The applicant is working with emerging designs of the adjacent Tesco site, in this context the rear block is acceptable in terms of massing and building distances. There should be a proposal in place for the ground level frontage of the podium to avoid creating inactive frontage onto the adjacent residential scheme and/or its future public realm.

46 In the scenario where the Tesco site remains as existing, there is concern for the residential quality of units looking west onto the Tesco building. Outlook appears poor for lower level units where the Tesco building feels relatively close. The ground floor courtyard proposed here is positive in terms of providing external space but the limited distance to the rear superstore wall limits the scope for high quality landscaping, access also appears restricted (through the refuse area). Increased distance here may enable landscaping solutions such as tree planting to create a buffer between the two building uses. Regard should also be given to the service units/AC etc. on the roof of Tesco, these will be viewed by middle level units and create noise pollution. Other existing building elements may cause conflicts, e.g. there are extractor fans/air vents along the east wall of Tesco. Sufficient distance for servicing either building without disruption to residents at this boundary should be accounted for.

47 Options to reduce issues of the west part of the building should be explored further, including shifting the theatre space and ground floor west to raise the level of the small courtyard and replace residential use with communal amenity on the 1st and 2nd floor area that faces onto the Tesco building; alternatively incorporating through-units here at least on the 1st and 2nd floors, where deck access may sit facing Tesco in order to have balconies and main outlook onto the larger courtyard. This may require the building element to be more slender which would help gain distance to the servicing and roof of Tesco.

Inclusive design

48 Inclusive design principles should be embedded into the development and design process from the outset. The aims of London Plan Policy 7.2 and draft London Plan Policy D3 are to ensure that proposals achieve the highest standards of accessibility and inclusion (not just the minimum). Policy 4.5 of the London Plan and Policy E10 of the draft London Plan seek to secure appropriate levels of accessible hotel accommodation. Specifically, Policy E10 of the draft London Plan outlines that 10% of rooms must be delivered as accessible units from the outset, or that 15% of new bedrooms are accessible rooms. Conditions of approval must be imposed to secure adherence to these standards of accessible accommodation.

Sustainable Development

Energy

49 The Energy Hierarchy has been followed and the proposed energy strategy is broadly supported; however, further revisions and information are required before the proposals can be considered acceptable to comply with Policies 5.2 and 5.13 of the London Plan as well as Policies S12 and S13 of the draft London Plan. Detailed comments have been forwarded to the LPA and the applicant under separate cover in this regard.

50 The non-residential carbon dioxide savings exceed the target set within Policy 5.2 of the London Plan. Notwithstanding this, residential carbon dioxide savings fall short of the on-site target within Policy 5.2 of the London Plan for the domestic uses. The applicant should therefore consider the scope for additional measures aimed at achieving further carbon reductions. Any remaining regulated carbon dioxide emissions must be met through a contribution to the borough's offset fund, secured in the s106 agreement.

51 The application site is within a district heating opportunity area and the scheme must provide commitment to ensure that the development is designed to allow future connection to a district heating network when one becomes available. This must include drawings demonstrating how the site is to be future-proofed for a connection to a district heating network in future. The applicant should propose a communal heat network supplied by a centralised energy centre. It should be confirmed that all apartments and non-domestic building uses will be connected to the heat network, and non-domestic uses will have capped-off connections. A drawing showing the route of the heat network linking all buildings/uses on the site should be provided.

52 The applicant is required to maximise the on-site savings from renewable energy technologies, regardless of the London Plan targets having been met. The applicant is proposing to install 37 kWp of Photovoltaic (PV) panels. They should confirm the area of net PV area and kWh of electricity generation. A roof layout has been provided, which suggests some top-floor roofs have been allocated to PV, however a more detailed roof layout is required demonstrating that the potential of all roofs for a PV installation has been maximised.

Flooding, drainage and green infrastructure

53 Detailed comments regarding flood risk and drainage have been forwarded to the Council and the applicant under separate cover. The surface water drainage strategy for the proposed development generally complies with Policy 5.13 of the London Plan and Policy S1.13 of the draft London Plan, and the inclusion of rainwater harvesting is particularly welcomed.

54 Insufficient information has been provided to determine if the proposed development meets the non-residential water consumption requirements of Policy 5.15 of the London Plan and S1.5 of the draft London Plan.

55 The Applicant should embed urban greening as a fundamental element of site and building design, in line with Policies 5.10 of the London Plan and G1 and G5 of the draft London Plan. Features such as street trees, green roofs, green walls, rain gardens, wild flower meadows, woodland and hedgerows should all be considered for inclusion. The Applicant should calculate the proposed development's Urban Greening Factor, as set out in Policy G5 of the draft London Plan, and aim to achieve the specified target.

Transport

Access, Delivery and Servicing

56 Public access to the cinema and ancillary gallery, workshops, café and bar is proposed via a main entry point on the western side of Station Road. Pedestrian entrances to residential dwellings will be provided via three separate entry points; two on Station Road and one on High Mead. These entry points are also proposed to be used by cyclists to access secured cycle stores located within the buildings. A new vehicular access route on High Mead will provide access to the disabled persons car parking area. This access route will also allow cyclists to use a small number of cinema-staff-only cycle parking spaces at the rear of the site.

57 Delivery and servicing for all facilities is proposed on-street, which is contrary to Policy T7F of the draft London Plan. In addition, the safety of pedestrians and cyclists is likely to be affected by the proposal for servicing vehicles to turnaround within the road. The applicant should therefore look at alternative provision for these functions on site. In the event that on-street delivery and servicing is unavoidable the applicant must demonstrate there is sufficient space within existing bays to accommodate the increased trips. This should be demonstrated alongside the provision of additional on-street disabled parking bays to demonstrate a worst case scenario can be accommodated satisfactorily.

Healthy Streets and Vision Zero

58 A Transport Assessment has been submitted with the application which generally accords with the requirements of Chapter 6 of the London Plan and Policies T1 and T2 of the draft London Plan. Whilst the Active Travel Zone section within the Transport Assessment and the accompanying survey report are welcomed, there is limited detail on how the development will deliver local improvements that supports the ten Healthy Streets Indicators as required by policy T2 of the draft London Plan.

59 The proposal has not demonstrated how it will positively contribute towards Vision Zero to actively address dangers on the local transport network. The development should promote 'Vision Zero for Road Danger' ensuring safety is at the forefront of the design, creating safe streets in line with Policy T2 of the draft London Plan.

Car parking

60 The scheme is a car-free development excluding three residential Blue Badge spaces. A new on-street car club space will be provided approximately 150m from the site, with the applicant offering three years free car club membership. In accordance with Policy T6 of the draft London Plan the three on-site Blue Badge spaces must be designed in accordance with the design guidance (BS0300 Vol.1), with spaces leased rather than sold. The proposed Electric Vehicle Charging Points (EVCP) should be secured by condition. Car club spaces and memberships must be secured through s106 legal agreement.

61 The Transport Assessment states that up to five further on-street Blue Badge spaces could also be accommodated if and when required approximately 20 metres west of the proposed High Mead pedestrian entrance to the residential dwelling. Concern is raised over the distance these proposed spaces are from the pedestrian entrance on Station Road. In addition, a requirement to fund provision of on-street EVCP for these spaces must be included in the s106 agreement. In addition, the applicant must demonstrate provision of non-residential disabled persons parking spaces in accordance with the standards expressed in Policy T6 of the draft London Plan. With such spaces located as close as reasonably practical to the front entrance of the cinema.

62 Parking should be secured by legal agreement and should be monitored, managed and enforced through a Parking Design and Management Plan which is adequately secured. As indicated by the Transport Assessment, a permit-free agreement should be secured in the s106 Agreement.

Trip Generation and approach to Highway and Public Transport Impact Assessment

63 The scheme proposes a car-free development, and it is reasonable to expect that the majority of new residential trips will be undertaken by public transport and active travel modes. The provided trip generation methodology and related analysis are considered unsuitable and require revision. The assessment should explore, preferably based on comparable TRICS data that is no more than five years old, the predicted net change in trips by different forms of transport that are likely to occur and discuss potential impacts on the transport network. A robust assessment should present and explain how the forecast trips have been split by journey purpose for relevant analysis periods, and how trips have been distributed to different modes of transport. Appropriate consideration should be given to the anticipated increases in the number of arrivals to the cinema and foreseeable impacts on the capacity of the public transport network because of the proposed development.

64 Contributions towards improvements and/or additional capacity for public transport may be required, subject to the findings of the revised Transport Assessment and trip generation analysis. Any such contributions must be secured in the s106 agreement.

Walking and Cycling

65 A total of 160 cycle parking spaces are proposed, including long and short stay spaces for residential units, as well as staff and visitor parking. This is in line with the draft London Plan requirements. Although the quantum of visitor parking is within policy requirements, the location and specification requires further consideration, as well as cyclist facilities, which must demonstrate compliance with the London Cycle Design Standards (LCDS).

66 Whilst the Active Travel Zone survey report included an extensive list of mitigation measures, the Transport Assessment does not indicate how the proposal is expected to contribute to encouraging walking and cycling in the vicinity of the site.

Travel Plan

67 It is welcomed that the applicant has submitted draft Travel Plans for both cinema and residential use to promote sustainable travel to and from the site. This outlines preliminary targets for mode shift for the residential and commercial uses, which accord with the Mayor's Transport Strategy. The finalised Travel Plans and all agreed measures and action plan should be secured, enforced, monitored and reviewed as part of the s106 agreement, in accordance with Policies 6.3 of the London Plan and T4 of the draft London Plan.

68 In accordance with London Plan Policies 6.3 and 6.14 and draft London Plan policy T7, a full Delivery and Servicing Plan, Construction Logistics Plan and the Travel Plan must be secured by condition.

Local planning authority's position

69 Council Officers from the London Borough of Harrow are continuing to assess the proposal. The proposal is intended to be presented to Council Committee in December 2019.

Legal considerations

70 Under the arrangements set out in Article 4 of the Town and Country Planning (Mayor of London) Order 2008, the Mayor is required to provide the local planning authority with a statement setting out whether he considers that the application complies with the London Plan, and his reasons for taking that view. Unless notified otherwise by the Mayor, the Council must consult the Mayor again under Article 5 of the Order if it subsequently resolves to make a draft decision on the application, in order that the Mayor may decide whether to allow the draft decision to proceed unchanged, or direct the Council under Article 6 of the Order to refuse the application, or issue a direction under Article 7 of the Order that he is to act as the local planning authority for the purpose of determining the application. There is no obligation at this present stage for the Mayor to indicate his intentions regarding a possible direction, and no such decision should be inferred from the Mayor's statement and comments.

Financial considerations

71 There are no financial considerations at this stage.

Conclusion

72 London Plan policies principle, housing, urban design, sustainable development and transport are relevant to this application. The below issues must be addressed to ensure the proposal complies with the London Plan:

- **Principle:** The principle of developing this site in the Harrow and Wealdstone Opportunity Area to provide 78 new homes and a replacement Cinema could be supported, in accordance with Policies 2.15, 2.3, and 3.3 of the London Plan as well as Policies H1, SD1, SD6 and SD7 of the draft London Plan, subject to further information on the re-provision and/or relocation of the existing church and improvements to the level of affordable housing proposed within the scheme.
- **Housing:** The affordable housing offer is 13%, all intermediate tenure, which is considered to be wholly unacceptable and should be significantly increased. There is no clear justification for the single tenure offer and all opportunities for the scheme to deliver affordable rent must be explored. GLA officers are currently robustly scrutinising the viability appraisal to maximise the provision of affordable housing. Early and late stage review mechanisms must be secured.
- **Urban design:** The layout, height, massing and appearance of the scheme are broadly supported in accordance with 7.5 of the London Plan and Policies D1 and D2 of the draft London Plan. Layout changes regarding courtyard and wheelchair access are required, as is further consideration of the relationship between the application site and adjacent Tesco site.
- **Sustainable Development:** Further revisions and information are required, and the carbon dioxide savings verified, before the energy proposals can be considered compliant with Policy 5.2 of the London Plan and Policy SI2 of the draft London Plan. Information demonstrating the development meets the non-residential water consumption requirements of Policy 5.15 of the London Plan and SI.5 of the draft London Plan is required. The Applicant should calculate the proposed development's Urban Greening Factor, as set out in Policy G5 of the draft London Plan, and aim to achieve the specified target.

- **Transport:** A revised transport assessment and trip generation assessment is required, demonstrating local improvements to achieve Healthy Streets and Vision Zero indicators required by Policy T2 of the draft London Plan. Delivery and servicing should be provided off-street, or demonstrated to function alongside additional on-street disabled parking bays. Blue badge parking, electric vehicle charging, car club spaces and memberships should all be secured by s106 agreement, along with improvements to public transport infrastructure where required. A parking design and management plan, delivery and servicing plan, construction logistics plan and revised travel plan must be secured by conditions of approval.

for further information, contact GLA Planning Unit (Development & Projects Team):

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APPENDIX 3: TRANSPORT FOR LONDON (TfL) CONSULTATION RESPONSE

Transport for London



Planning Ref: 19/217/HRRW - Draft Stage 1 LPA Comments
TfL Spatial Planning Ref: HRRW/19/217

Katie Parkins
London Borough of Harrow
-Sent via email only-

Transport for London
City Planning

5 Endeavour Square
Westfield Avenue
Stratford
London E20 1JN

Phone 020 7222 5600
www.tfl.gov.uk

25th November 2019

Dear Katie,

Re: Safari Cinema, Station Road, HA1 2TU, Harrow - TfL's Initial Response

I write following receipt of the above planning application reference P/3043/19 to expand on the issues raised in the Stage 1 report dated 7th November 2019. This letter sets out TfL's additional detailed comments with regards to the development proposals.

This response relates to an application for 'redevelopment to provide Part 11/Part 5 storey building with basement level comprising of 78 residential units (3 X studios) (17 X 1 bed) (43 X 2 bed) (15 X 3 bed); cinema (Use class D2) to ground floor and ancillary cafe to first and second floors; parking; landscaping; bin and cycle stores'

Please note that the following comments represent the view of TfL and are made entirely on a 'without prejudice' basis. They should not be taken to represent an indication of any subsequent Mayoral decision in relation to a planning application based on the proposed scheme. In addition, these comments do not necessarily represent the views of the Greater London Authority (GLA).

Site Description and Context

The redevelopment site is currently occupied by Safari Cinema and is bounded by High Mead to the north, Station Road to the east and commercial / retail to the south and west. The site is situated on the western side of Station Road, a key bus corridor that operates with a 30mph speed limit.

There are a variety of bus routes operating along Station Road with daytime frequencies of up to 12 buses per hour. The nearest bus stops serve northbound services opposite to the site and southbound services approximately 200m of the site. Harrow & Wealdstone railway station is located approximately 650m north of the site. It is served by London Underground (Bakerloo Line), London Overground and the West Midlands and Southern railways.

MAYOR OF LONDON



VAT number 756 2769 90

Consequently, the site has a very good Public Transport Access Level (PTAL) of 5, on a scale of 1 – 6 where 6 is the best.

Site Access

Public access to the cinema and ancillary gallery, workshops, café and bar is proposed via a main entry point on the western side of Station Road, leading directly into the foyer.

Pedestrian entrances to residential dwellings will be provided via three separate entry points; two on Station Road and one on High Mead, adjacent to a proposed shared driveway at the rear of the site. These entry points are also proposed to be used by cyclists to access secured cycle stores located within the buildings.

A new vehicular access route on High Mead will provide access to the disabled persons car parking area. This access route will also allow cyclists to use a small number of cinema-staff-only cycle parking spaces at the rear of the site.

Refuse residential/commercial collection is intended to take place on-street in accordance with existing arrangements along High Mead.

Delivery and Servicing

Delivery and servicing for all facilities is proposed on-street, which is contrary to draft London Plan (2019) policy T7F. In addition, there are concerns over the safety of pedestrians and cyclists regarding the proposal for servicing vehicles to turnaround within the road. The applicant is encouraged to look at alternative provision on site. Nevertheless, if on-street provision is proposed, the applicant should demonstrate there is sufficient space within existing bays to accommodate the increased trips. In addition, this should be demonstrated alongside the provision of the additional on-street disabled parking bays proposed to show a 'worse case' scenario can be accommodated satisfactorily.

Healthy Streets and Vision Zero

In accordance with draft London Plan (2019) policy T2, Healthy Streets, the applicant must ensure their proposal delivers a scheme that meets aims to improve air quality, reduce congestion and make attractive places to live, work and do business. Whilst the Active Travel Zone (ATZ) section within the Transport Assessment (TA) and the ATZ survey report are welcomed, there is limited detail on how the development will deliver local improvements that supports the ten Healthy Streets Indicators.

The development should promote 'Vision Zero for Road Danger' ensuring safety is at the forefront of the design, creating safe streets in line with Policy 3 of the draft London Plan (2019). The proposal has not demonstrated how it will positively

contribute towards Vision Zero to actively address dangers on the local transport network. As clarification is required, this proposal cannot be fully supported.

Car Parking

The scheme is for a car-free development except for three residential Blue Badge spaces. A new on-street car club space will be provided approximately 150m from the site, with the applicant offering three years free car club membership that should be secured through an appropriate legal agreement.

As per the draft London Plan (2019) policy T6, the three on-site Blue Badge spaces must be designed in accordance with the design guidance, with spaces leased rather than sold adhering to policy. The proposed Electric Vehicle Charging Points (EVCP) should be secured by condition.

The TA also states that up to five further on-street Blue Badge spaces could also be accommodated if and when required approximately 20m west of the proposed High Mead pedestrian entrance to the residential dwelling. Concern is raised over the distance these proposed spaces are from the pedestrian entrance on Station Road. In addition, a requirement to fund provision of on-street EVCP for these spaces must be included.

The TA does not refer to any provision of a disabled persons parking space, as per non-residential disabled persons parking standards within the draft London Plan (2019) policy T6. This provision must be incorporated from the planning stage. It should also be located as close as possible to the main entry point to the cinema.

Parking should be secured by legal agreement and should be monitored, managed and enforced through a Parking Design and Management Plan. As indicated by the TA, a permit-free agreement should be secured in the s106 Agreement.

Trip Generation and Approach to Highway and Public Transport Impact Assessment

As it currently stands, the trip generation methodology and related analysis are considered unsuitable. The assessment should explore, preferably based on comparable TRICS data that is no more than five years old, the predicted net change in trips by different forms of transport that are likely to occur and discuss potential impacts on the transport network.

This consideration is important, as the scheme proposes a car-free development, and it is reasonable to expect that the majority of new residential trips will be undertaken by public transport and active travel modes. A robust assessment should present and explain how the forecast trips have been split by journey purpose for relevant analysis periods, and how trips have been distributed to different modes of transport.

Whilst it is reasonable to expect an overall reduction of visitor numbers to the site related to the congregation on service days (church use), appropriate consideration should be given to the anticipated increases in the number of arrivals to the cinema and foreseeable impacts on the capacity of the public transport network because of the proposed development.

Contribution towards improvements/additional capacity for public transport may be required, subject to the findings of the TA and trip generation. A decision will be made upon assessing the transport assessment produced when the formal application has been submitted.

Cycle Parking

A total of 160 cycle parking spaces are proposed, including long and short stay spaces for residential units, as well as staff and visitor parking. This comprises 145 long stay residential and three long stay non-residential spaces, as well as two short stay residential spaces and 10 spaces for visitors to the cinema. This is in line with the draft London Plan (2019) requirements.

Although the quantum of visitor parking is within policy requirements, the location and specification require further consideration, as well as cyclist facilities.

Long stay cycle spaces for the residential element will be located in three separate cycle stores on the ground floor and one in the basement accessible via a lift. As previously indicated, cinema-staff-only cycle parking spaces will be located at the rear of the site.

Proposed cycle parking spaces for cinema visitors will be placed on Station Road in the form of five Sheffield stands. An initial concern about the location of these five stands has been raised because of the use of space. This concern is related to the ability of these stands to adequately cater for larger cycles whilst preventing conflict with refuse collection activities that will take place in the adjacent resident refuse storage area. It is recommended that the location for the stands be reviewed.

Two-tier stands tend not to be suitable for all users and all types of cycles. Provision of other types of stand should be considered. Provision of showers, lockers and changing facilities for cyclists associated to commercial uses should also be considered. The finalised details of cycle parking should be secured by condition.

Walking & Cycling

Whilst the ATZ survey report included an extensive list of mitigation measures, the TA falls short to indicate how the proposal is expected to contribute to encouraging walking and cycling in the vicinity of the site.

Measures could consider improved walkability with public transport and the delivery of cycling infrastructure, including signage and on street cycling facilities. A clear action plan with the local planning and highway authority should be agreed to ensure improvements make the wider area more attractive for walking and cycling and better connect the site.

Travel Plan

It is welcomed that the applicant has submitted draft Travel Plans (TP) for both cinema and residential use to promote sustainable travel to and from the site. This outlines preliminary targets for mode shift for the residential and commercial uses, which accord with the Mayor's Transport Strategy. The finalised TPs and all agreed measures and action plan should be secured, enforced, monitored and reviewed as part of the s106 agreement, in accordance with draft London Plan (2019) Policy 6.3 and Policy T4.

Delivery & Servicing Plan and Construction Logistics Plan

The applicant has submitted a draft Delivery and Servicing Plan (DSP) and Construction Logistics Plan (CLP), which adopts the recommended structure as described by TfL guidance and is considered largely acceptable at this planning stage, in line with the draft London Plan (2019) Policies 6.3 and 6.14 and Policy T7.

The CLP should carefully consider, inter alia, safe turning provision for vehicles associated with temporary loading/unloading bays and how any impacts on the bus operations and passengers at the footway and layby at the front of the building will be mitigated. A full DSP and CLP need to be secured by condition.

Mayoral Community Infrastructure Levy

The Mayor of London introduced his Community Infrastructure Levy 2 (MCIL2) on 1st April 2019. The proposed development is located in Harrow where the charging rate is £60 per square metre of floorspace.

Summary

Further information is required regarding the assessment and contributions towards Healthy Streets and Vision Zero. The location and specification of cycle parking require further consideration, as well as cyclist facilities. Concern is raised over the suitability and quantum of disabled parking, and the location and safety of on-street servicing.

Notwithstanding the specific comments discussed above, we would like to draw to your attention the following key issues:

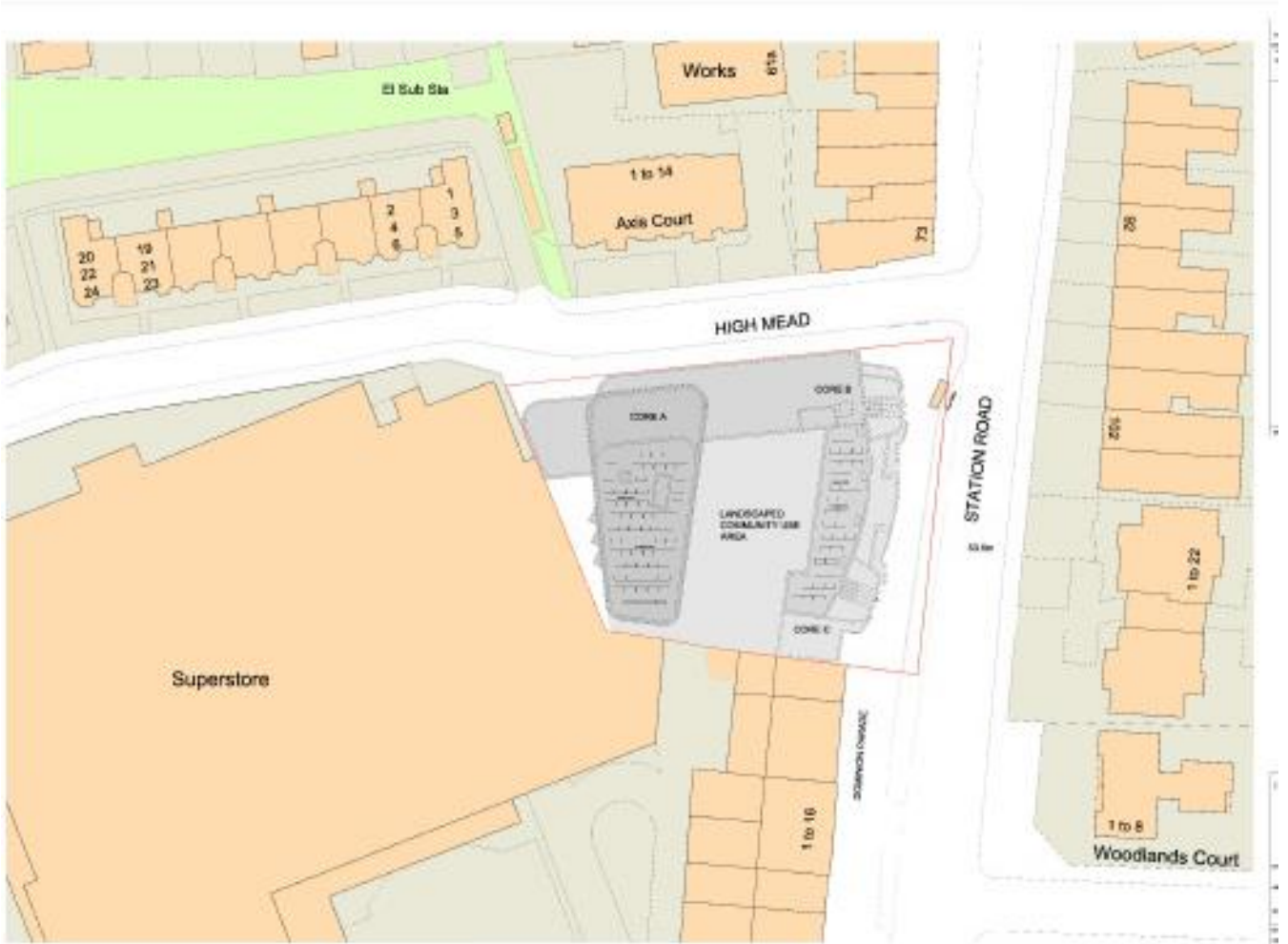
- Servicing provision: The scheme should demonstrate there is sufficient space to support the adequate operation within existing loading bays on Station Road to accommodate large vehicles related to increased trips and prevent any overflow into the bus stop. It should also demonstrate there is sufficient space on High Mead to accommodate the proposed 8m loading bay alongside an additional on-street disabled persons parking bay, as per non-residential disabled persons parking standards, and on-street Blue Badge spaces in future upon request.
- As it is reasonable to expect the proposed development will add pedestrian and cyclist movements around the area, clarification is required in relation to mitigation measures or improvement commitments that support the ten Healthy Streets indicators and positively contribute towards Vision Zero to actively address dangers on the local transport network.
- As per the Economic Statement submitted with the planning application, appropriate consideration should be given to the anticipated increases in the number of arrivals to the cinema and foreseeable impacts on the capacity of the public transport network because of the proposed development. Contribution towards improvements/additional capacity for public transport may be required.
- Proposed cycle parking spaces for cinema visitors will be placed on Station Road in the form of five Sheffield stands. An initial concern about the location of these five stands has been raised because of the use of space. This concern is related to the ability of these stands to adequately cater for larger cycles whilst preventing conflict with refuse collection activities that will take place in the adjacent resident refuse storage area. It is recommended that the location for these stands be reviewed.
- Two-tier stands tend not to be suitable for all users and all types of cycles. Provision of other types of stand should be considered. Provision of showers, lockers and changing facilities for cyclists associated to commercial uses should also be considered.

I trust that the above provides you with a better understanding of TfL's current position on the application. Please do not hesitate to contact me if you would like to discuss further any of the issues raised above.

Yours sincerely,

Juan Sanclemente
Principal Technical Planner
Email: V_JuanSanclemente@tfl.gov.uk

APPENDIX 4: SITE PLAN



APPENDIX 5: PHOTOGRAPHS



Application site when viewed from the south.



North elevation of the host building



North elevation of the host building, adjacent to Tesco



Dwellings along High Mead

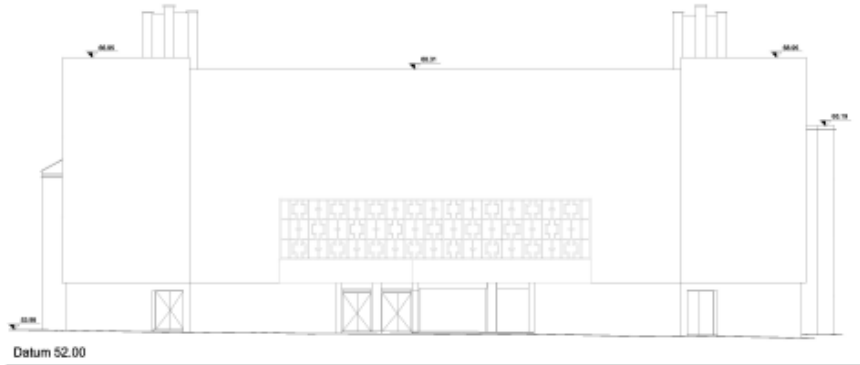


Dominion Parade located to the south



APPENDIX 6: PLANS AND ELEVATIONS

Existing Elevations



Elevation 1
EXISTING STATION ROAD ELEVATION



Elevation 2
EXISTING HIGH MEAD ELEVATION

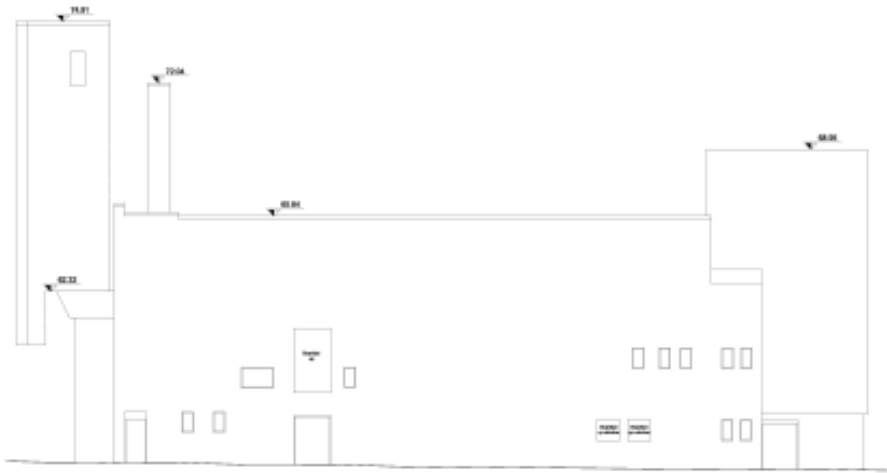




Datum 52.00

Elevation 3

EXISTING WEST ELEVATION



Datum 52.00

Elevation 4

EXISTING SOUTH ELEVATION

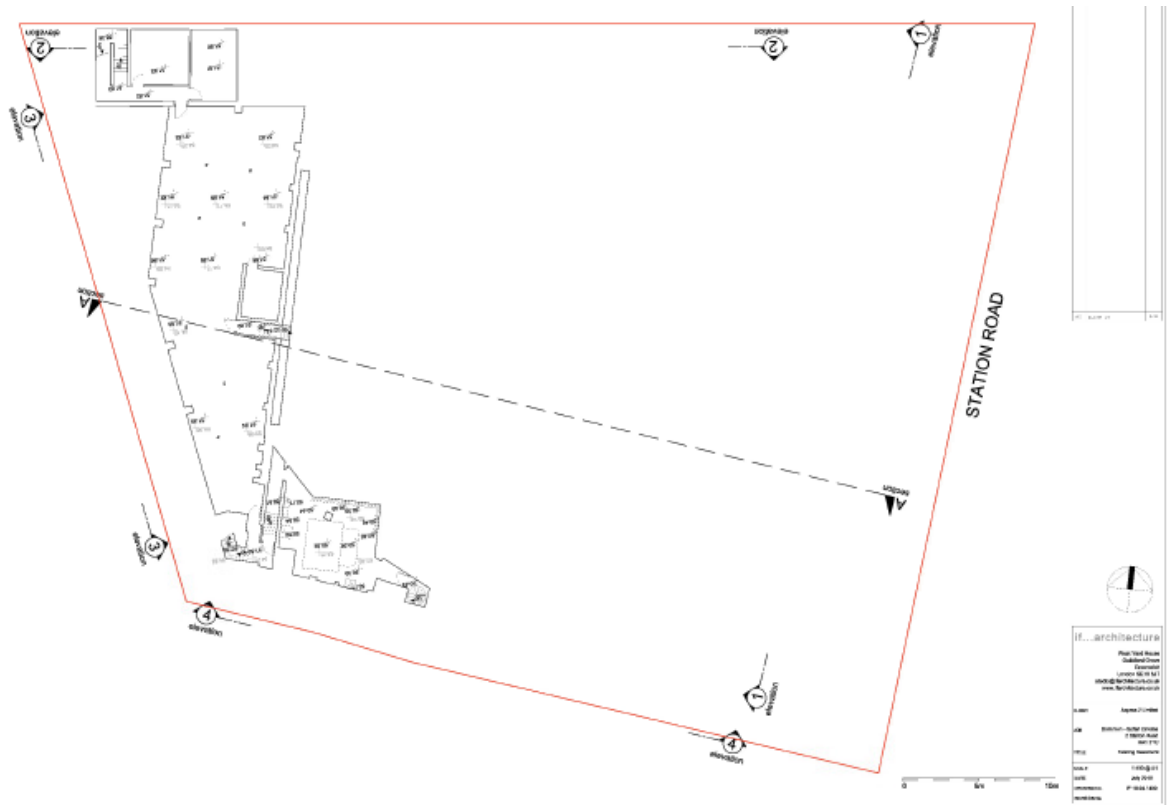




Datum 52.00

EXISTING HERITAGE ELEVATION

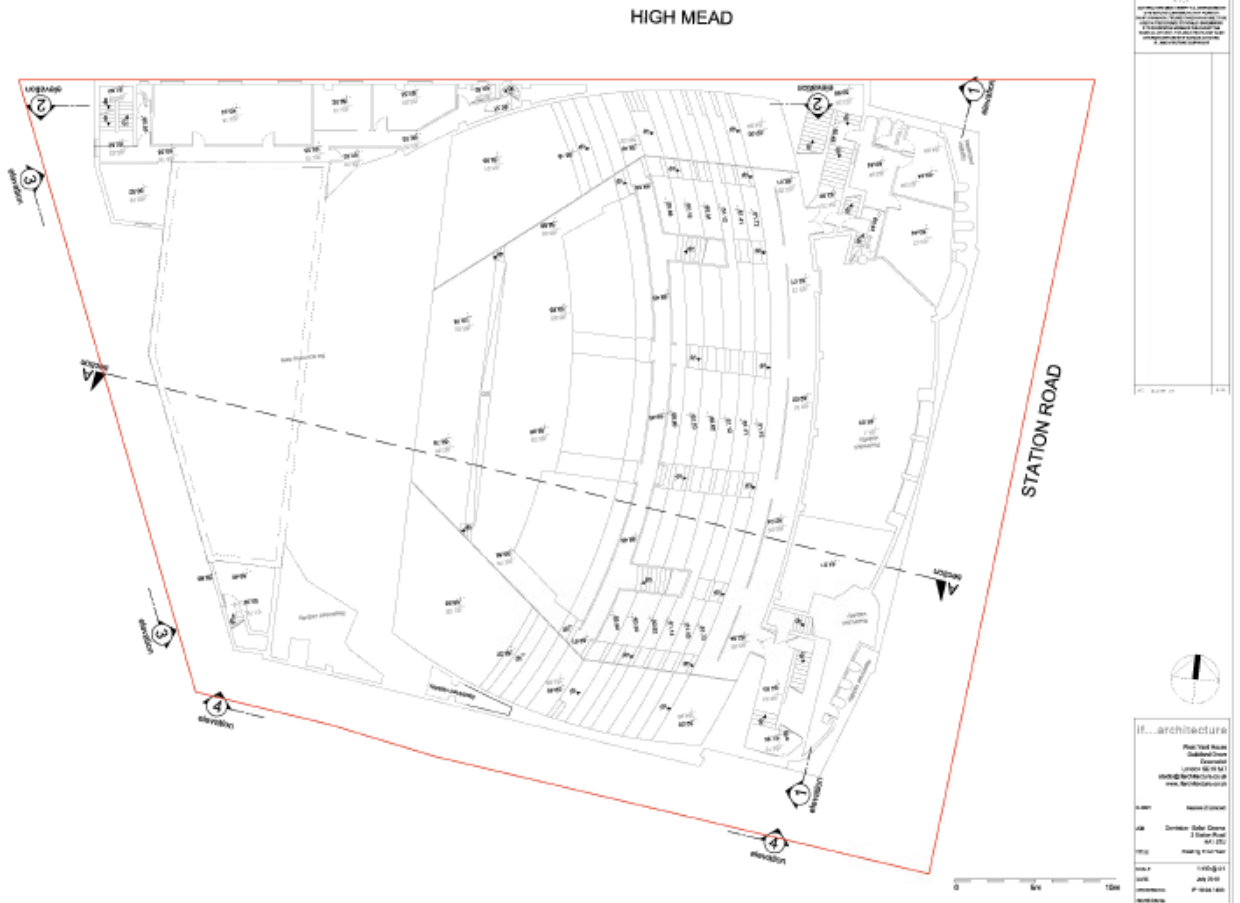
Existing Floorplans



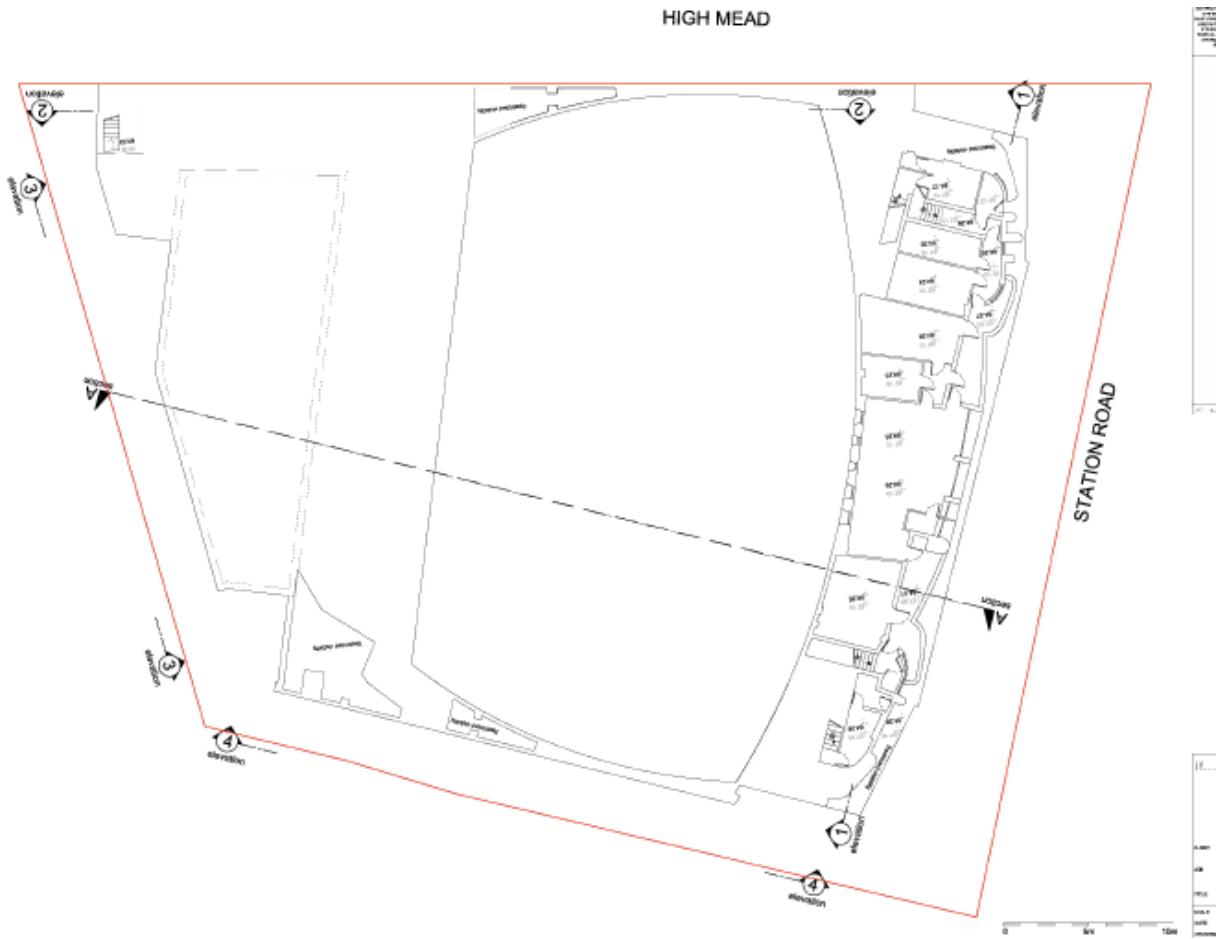
Existing Basement Plan



Existing Ground Floor Plan

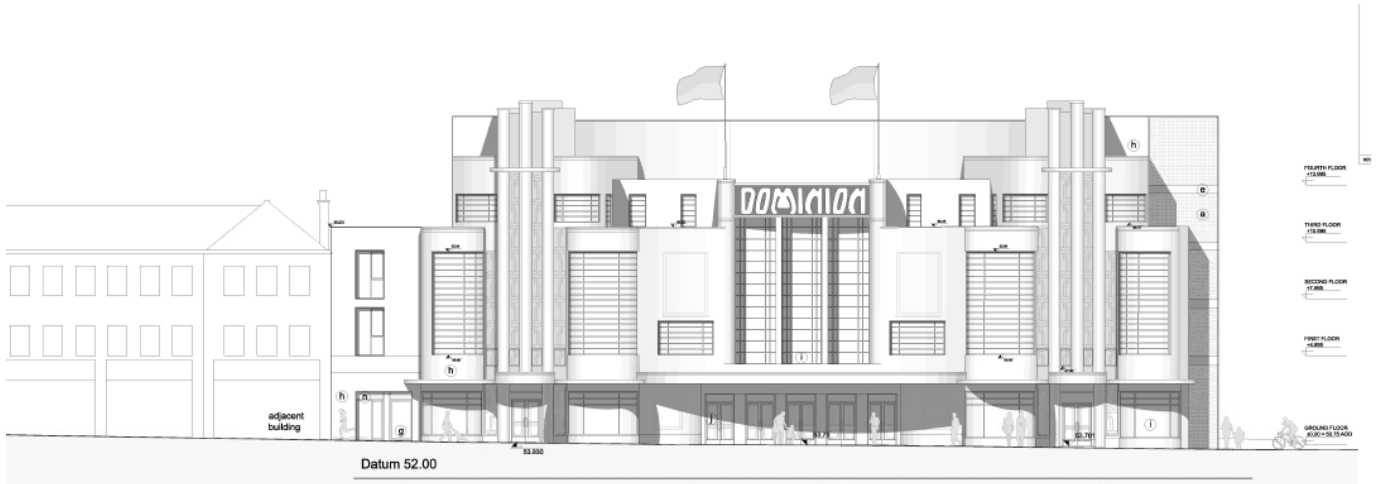


Existing First Floor Plan



Existing Second Floor Plan

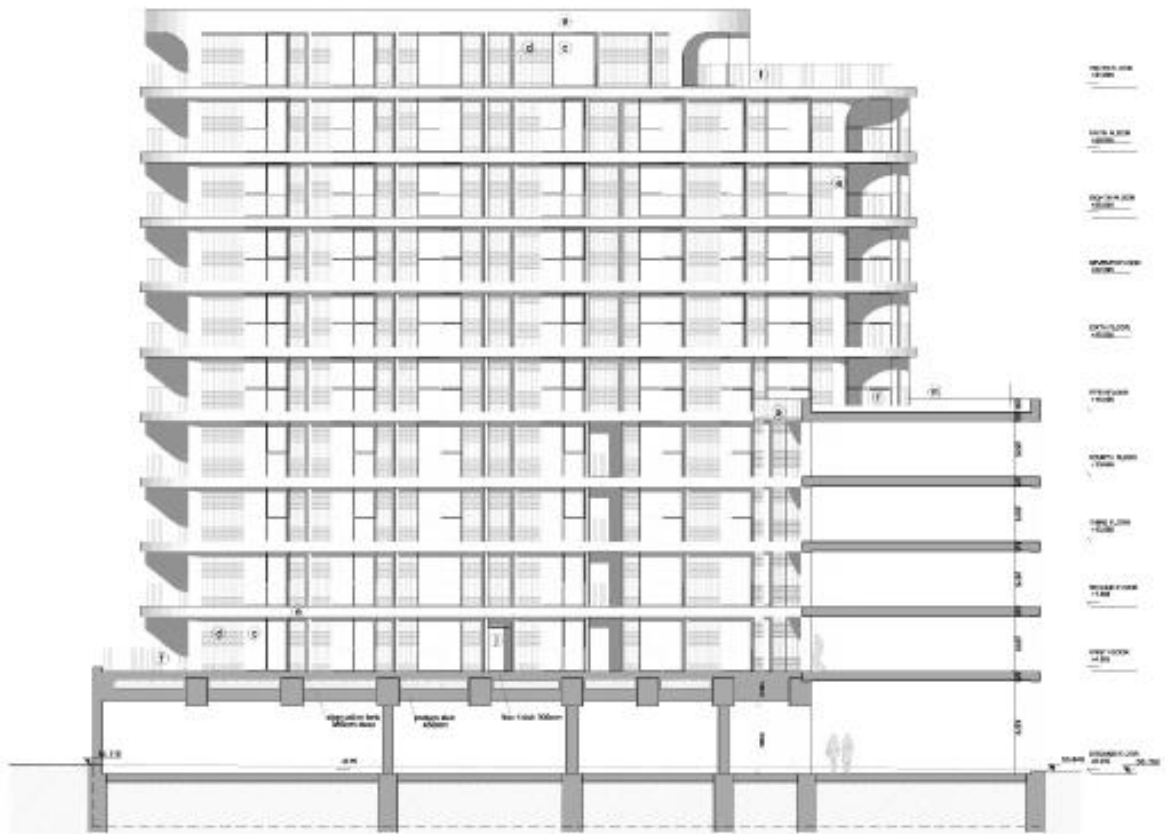
Proposed Elevations



Proposed Heritage Elevation



Proposed North Elevation



East Elevation / Section C-C

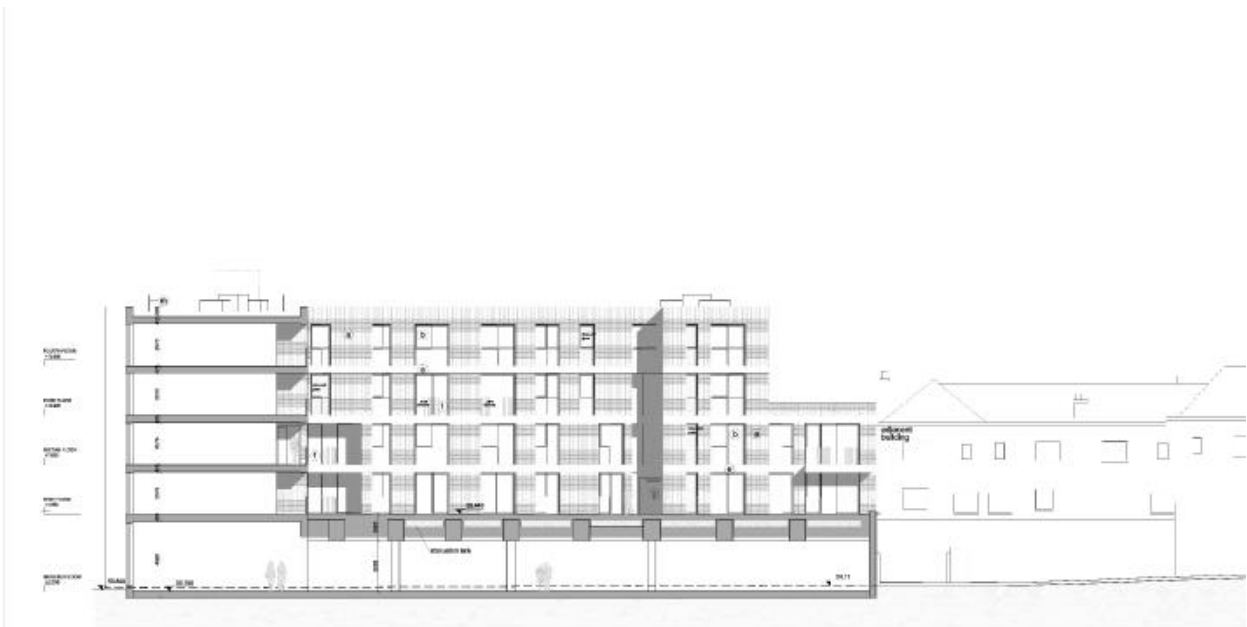
Proposed East Elevation



Proposed South Elevation /Section

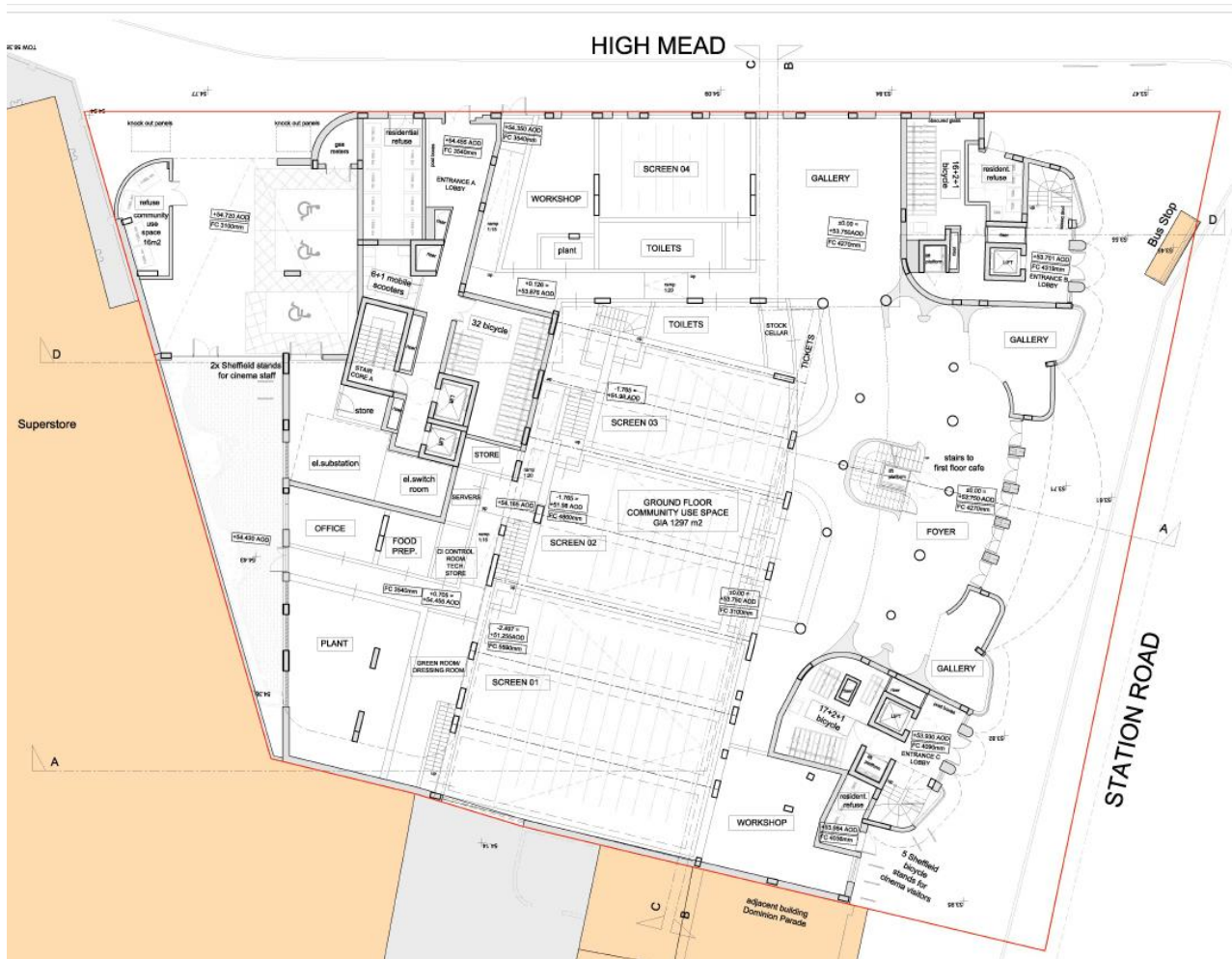


Proposed West Elevation



Proposed West Elevation

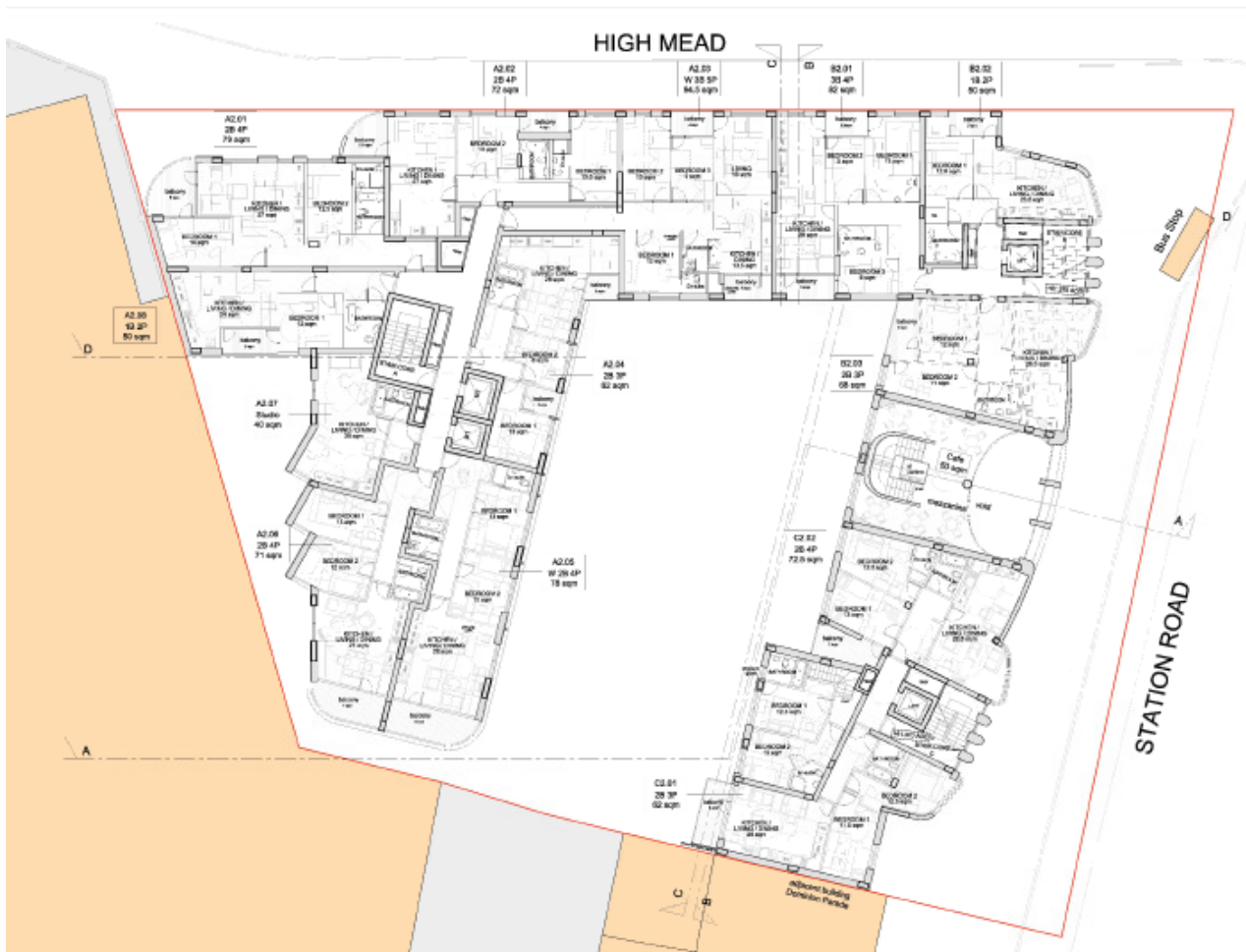
Proposed Floorplans



Proposed Ground Floor Plan



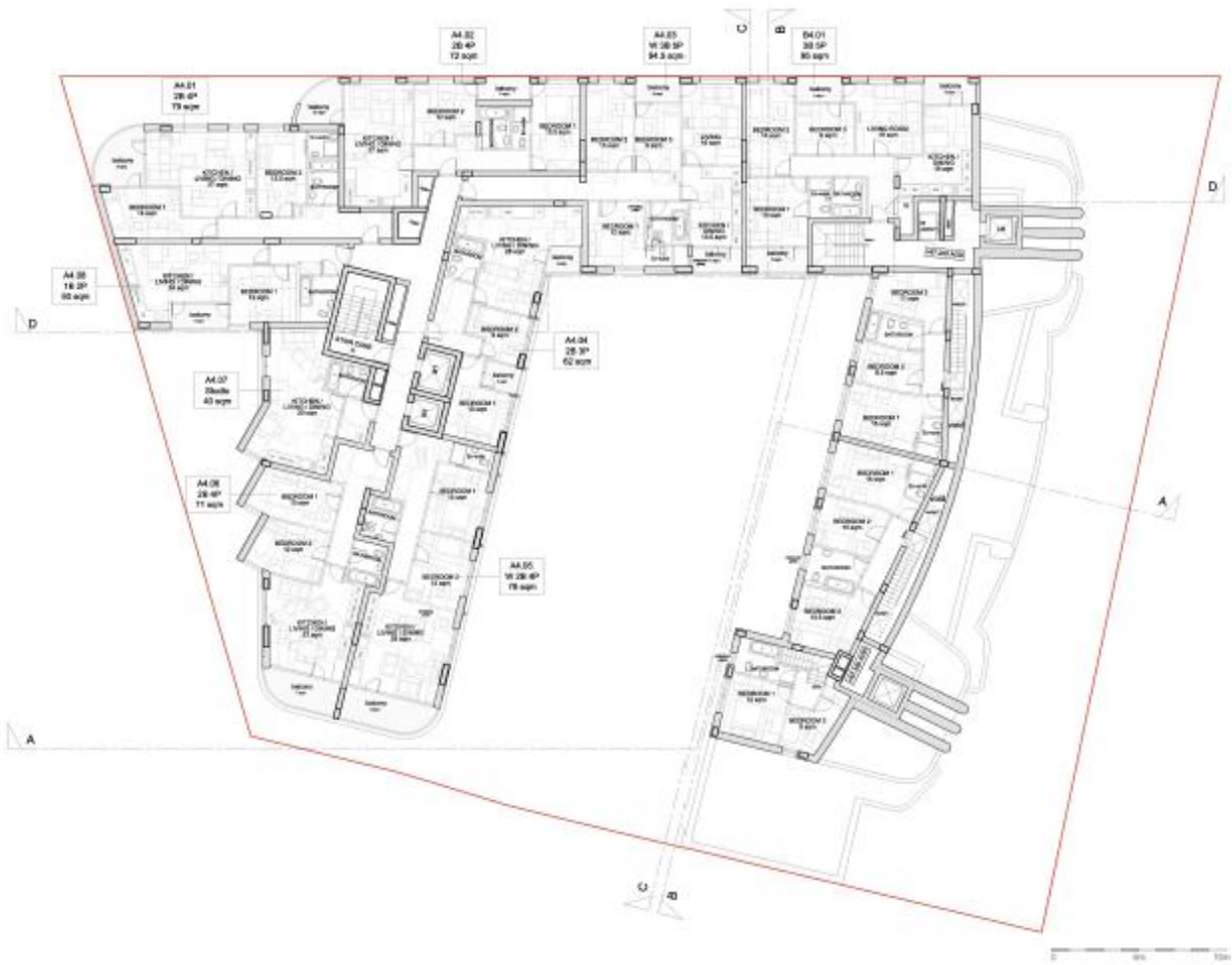
Proposed First Floor Plan



Proposed Second Floor Plan



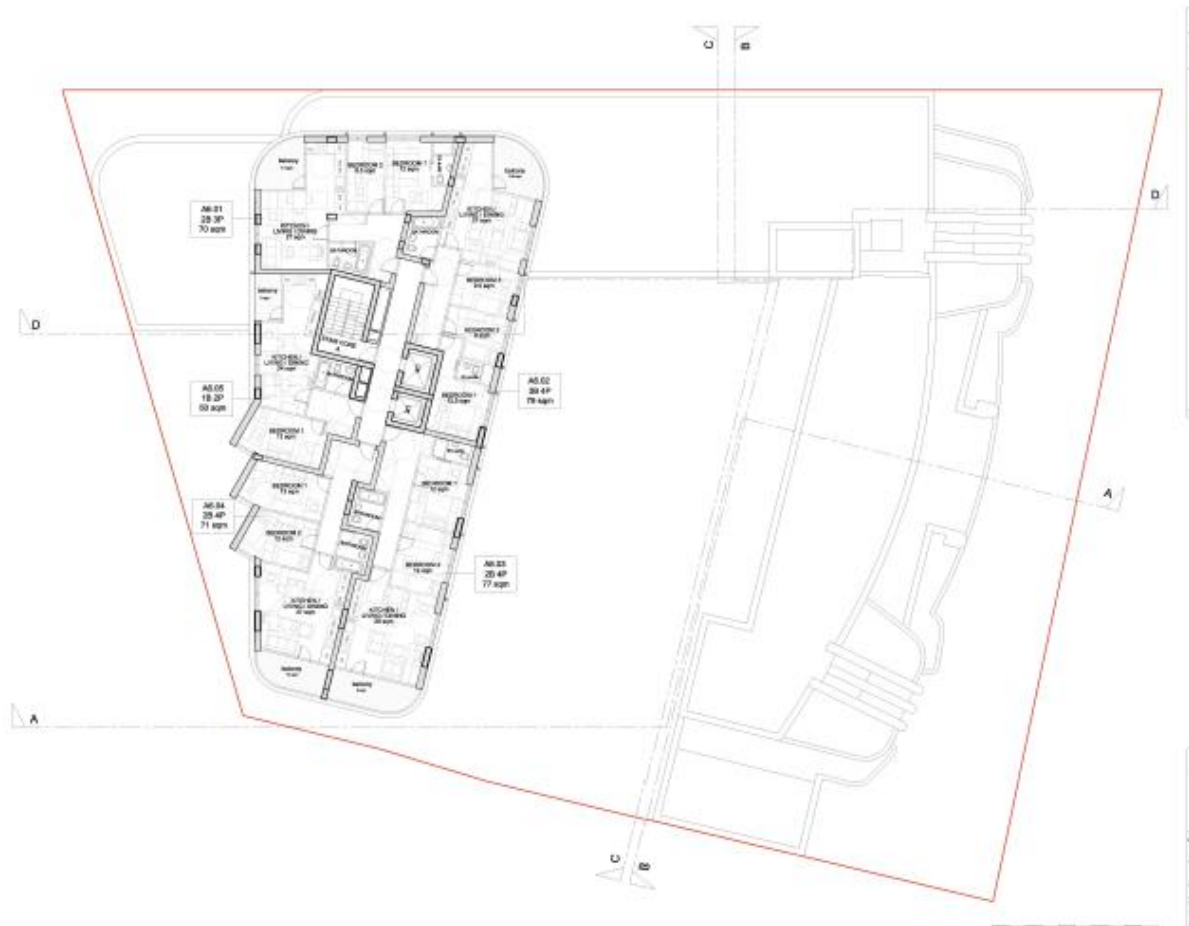
Proposed Third Floor Plan



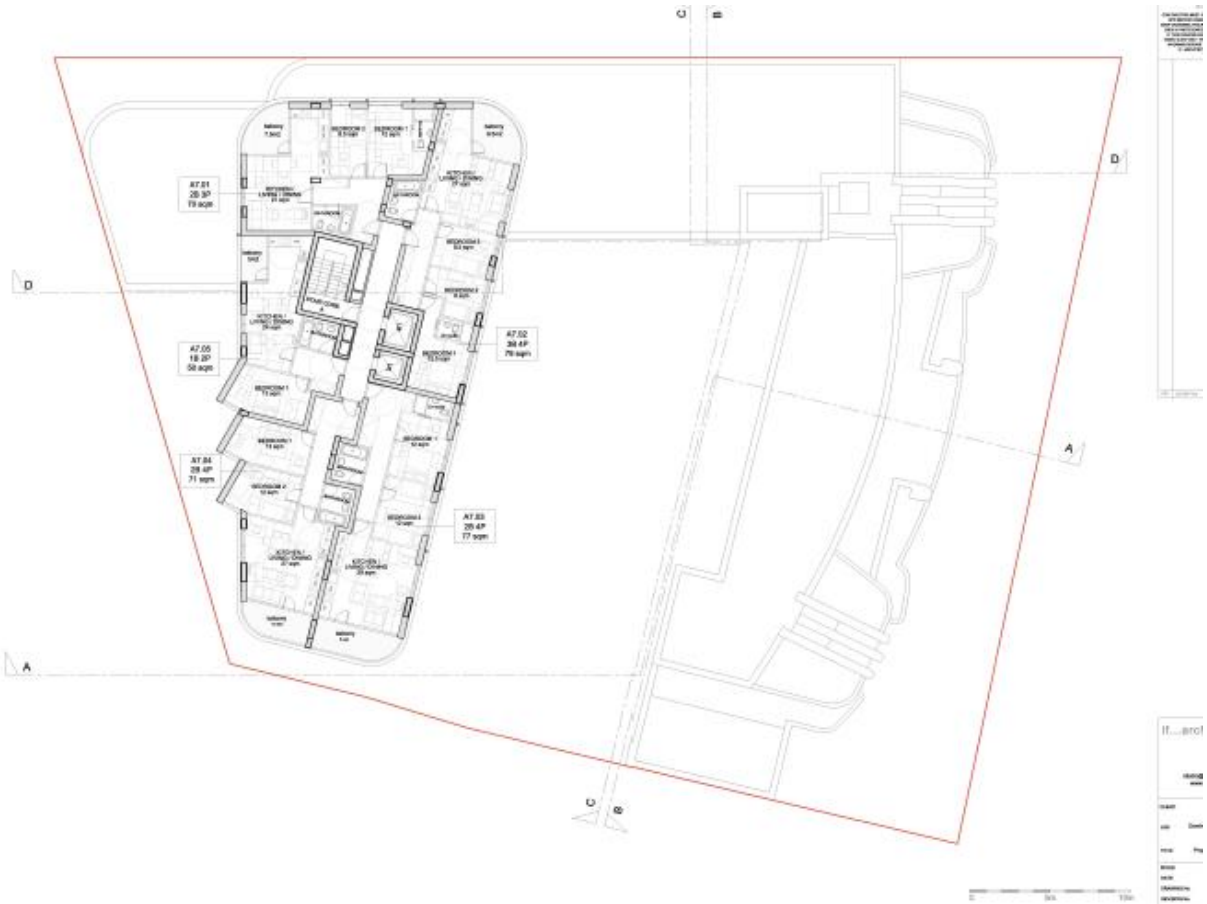
Proposed Fourth Floor Plan



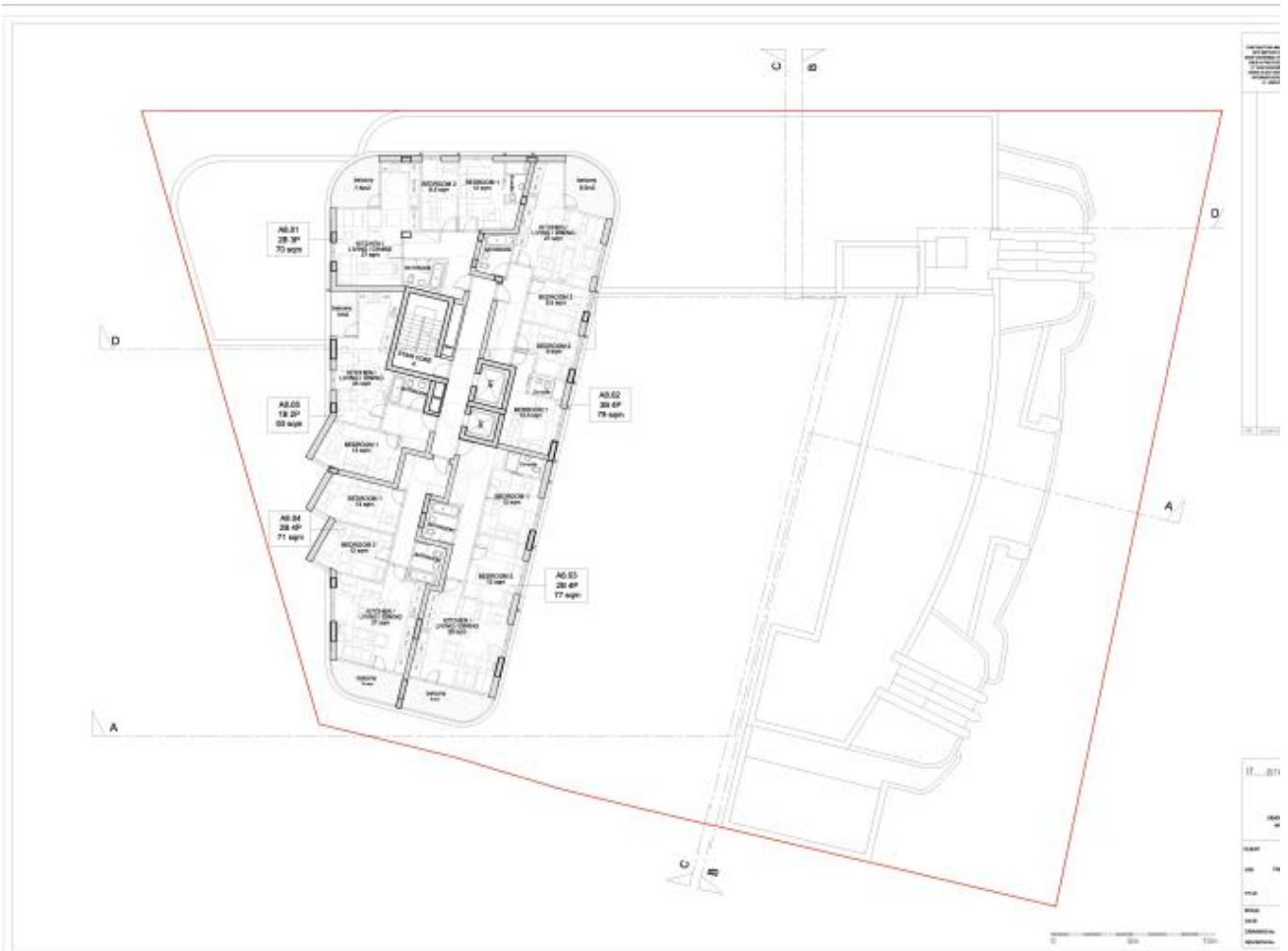
Proposed Fifth Floor Plan



Proposed Sixth Floor Plan



Proposed Seventh Floor Plan



Proposed Eighth Floor Plan

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